



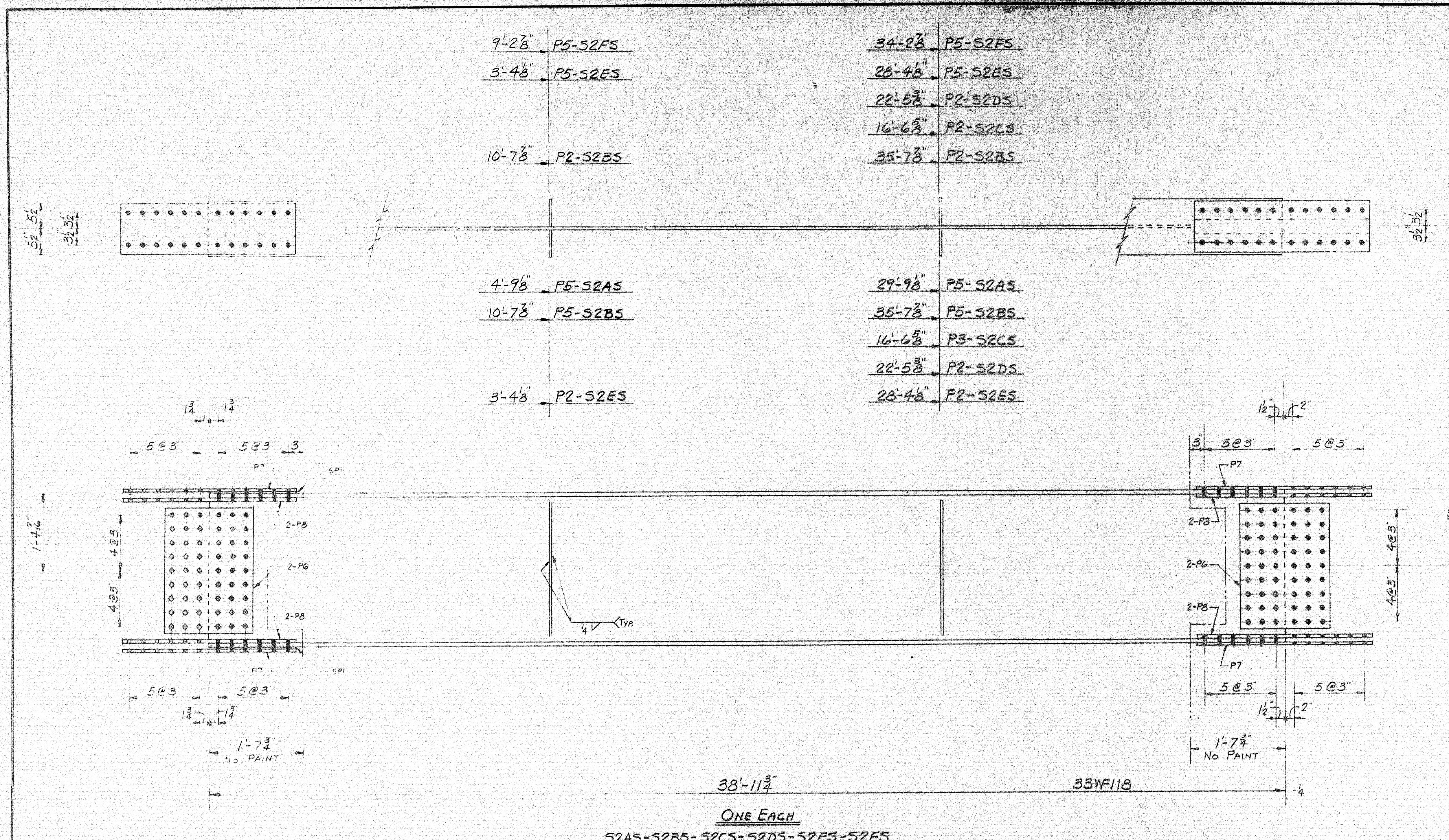


SHIP		BILL OF MATERIAL				DWG. B66-399-59
MARK	NO.	MARK	SHAPE	LENGTH	WT.	REMARKS
S2AS	1		33WF118	38'11 3/4"		A441
S2BS	1			38'11 3/4"		
S2CS	1			38'11 3/4"		
S2DS	1			38'11 3/4"		
S2ES	1			38'11 3/4"		
S2FS	1		do	38'11 3/4"		do
	7	P2	R6x3	2 5		A36
	1	P3		2 5		
	8	P5	do	2 5		
	24	P6	R10 1/2 x 3/8	2 3		
	24	P7	R11 x 1/2	3 0 2		
	48	P8	R4 x 3/4	3 0 2		
	12	SPI	R11 x 3/8	1 6 4		do
	96	SHOP	3/8 H.S. BOLT	0 3 2		FLANGE BOLTS A325
	24	SHOP	do	0 3 4		WEB BOLTS "
FIELD	495		3/8 H.S. BOLT	0 3 2		FLANGE A325
do	636		do	0 3 4		WEB do
do	1250		3/8 H.S. WASHER			
do	224		3/8 M. BOLT	0 2		
do	448		3/8 STD. WASHER			

SHOP CONNECTIONS: WELD E70 LOW-HYDROGEN  
 FIELD CONNECTIONS: 3/8 H.S. BOLTS  
 HOLES: 1/8" U.N.  
 PAINT: STATE OF MAINE SPECS

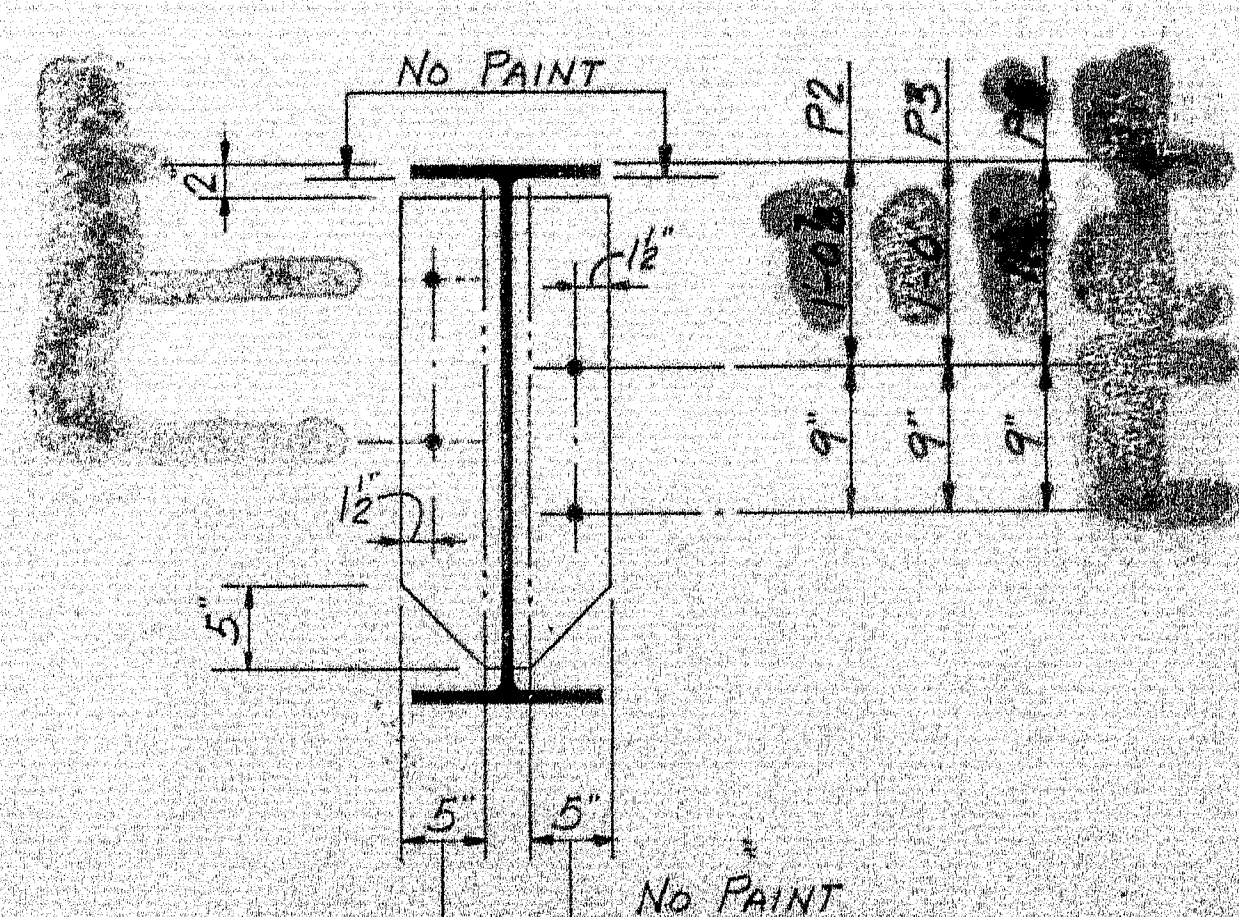
Proj. No. I-95-9(34) 258		APP. AS NOTED 2-13-67	
SOUTHBOUND		STRINGERS SPAN #2	
PRINT ISSUE		Bancroft & Martin Inc. Brewer, Maine	
2 S.H.C. 2-15-67		I-95 OVER BENEDICTA ROAD	
3 CUST. 2-15-67		SHERMAN, MAINE	
6 SHOP 2-15-67		CUSTOMER CALLAHAN BROS.	
3 F.A. 1-30-67		DESIGNER M.S.H.C.	
DRAWN 1-27-67 C.J.M.		ORDER VERBAL	
REVISION		DWG. B66-399-59	
REVISION			

100-137



PLACE ANY NATURAL CAMBER UP

MATCH THIS END WITH STRINGERS MARKED "SI"

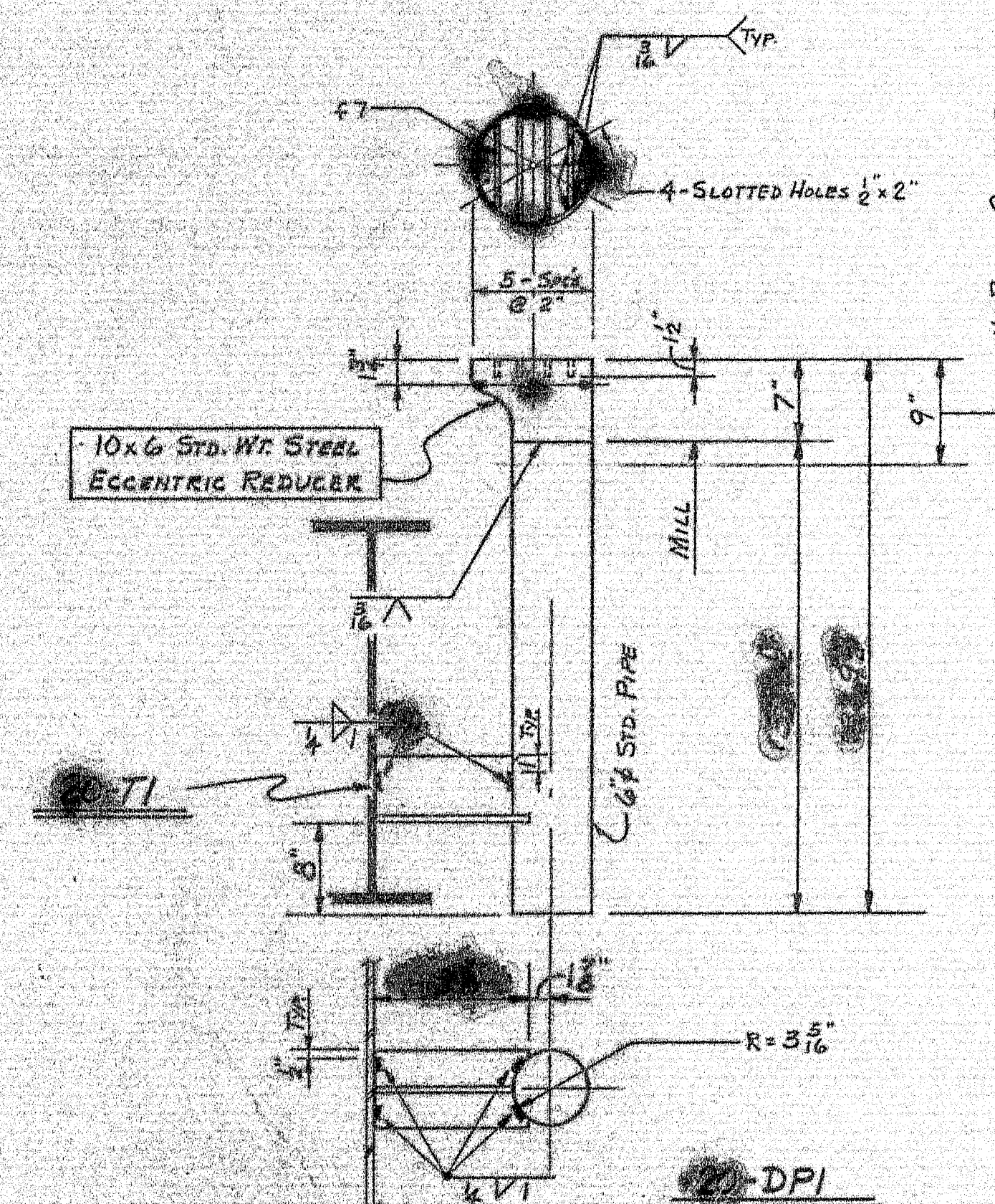
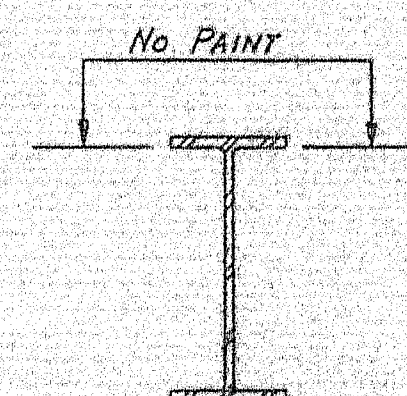
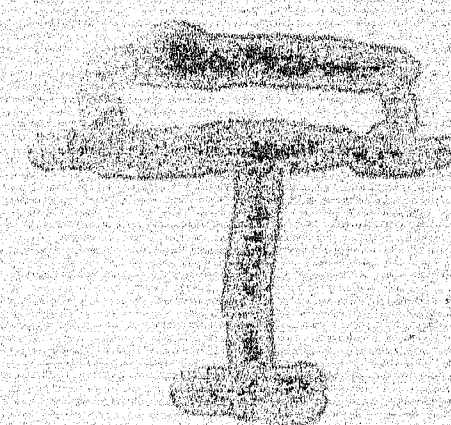
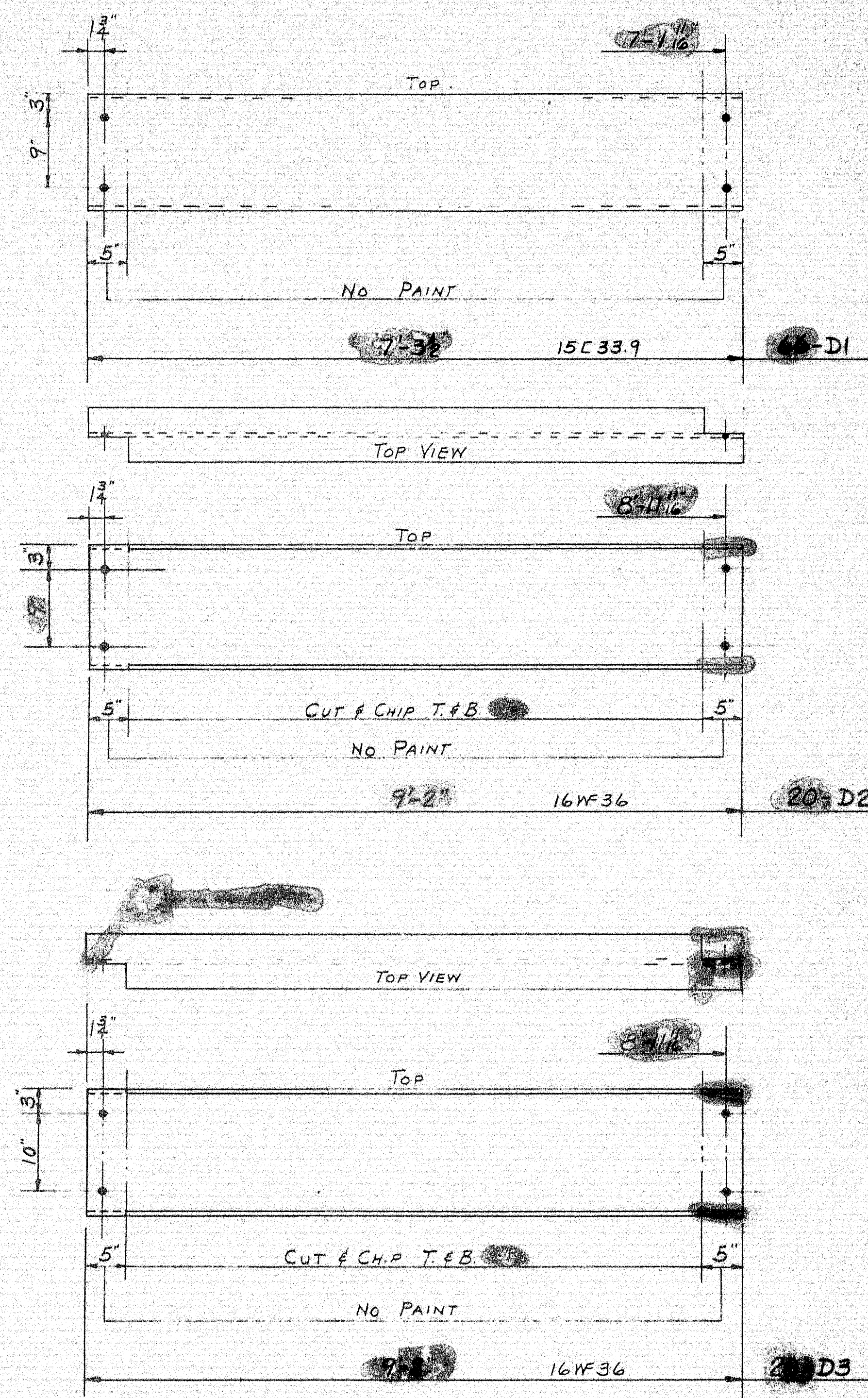


MATCH THIS END WITH STRINGERS MARKED "S3"









SHIP		BILL OF MATERIAL				DWG.
MARK	NO.	MARK	SHAPE	LENGTH	WT.	REMARKS
D1	20		15C33.9	7'-1 1/2"		A36
D2	20		16WF36	9'-2"		
D3	20		do			
DPI	20		60 STD. PIPE	3'-2 1/2"		MIE A120
			10x6 STD. STEEL WELD ECCENTRIC REDUCER			Req. Do
T1	20		576WF135	0'-9 3/4"		A36
		F7	FB 1 1/2 x 1/2	0'-10"		CUT TO FIT

SHOP CONNECTIONS: WELD (E-70 LOW-HYDROGEN)  
 FIELD CONNECTIONS: BOLT & WELD  
 HOLES: 15/16" UNLESS NOTED  
 PAINT: STATE OF MAINE SPECS.

PRJ. NO. I-95-9		DIAPHRAGMS & DRAIN PIPE	
PRINT ISSUE		Bancroft & Martin Inc. Brewer, Maine	
5	S.H.C.	5	DESIGNED BY
5	CUST.	5	CHECKED BY
5	SHOP	5	DATE
5	F.A.	5	DATE
DRAWN	G.J.M.	CUSTOMER	STATE HIGHWAY COMMISSION
REVISION		DESIGNER	STATE HIGHWAY COMMISSION
REVISION		ORDER	VERBAL
REVISION		DWG.	

100-139



### INDEX OF SHEETS

Sheet No.	Title
1	General Plan and Quantities
2	Foundation Survey
3	Abutment No. 1 Southbound
4	Abutment No. 2 Southbound
5	Abutment No. 1 Northbound
6	Abutment No. 2 Northbound
7	Structural Steel
8	Superstructure Details
9	Superstructure Details
10	Reinforcing Steel

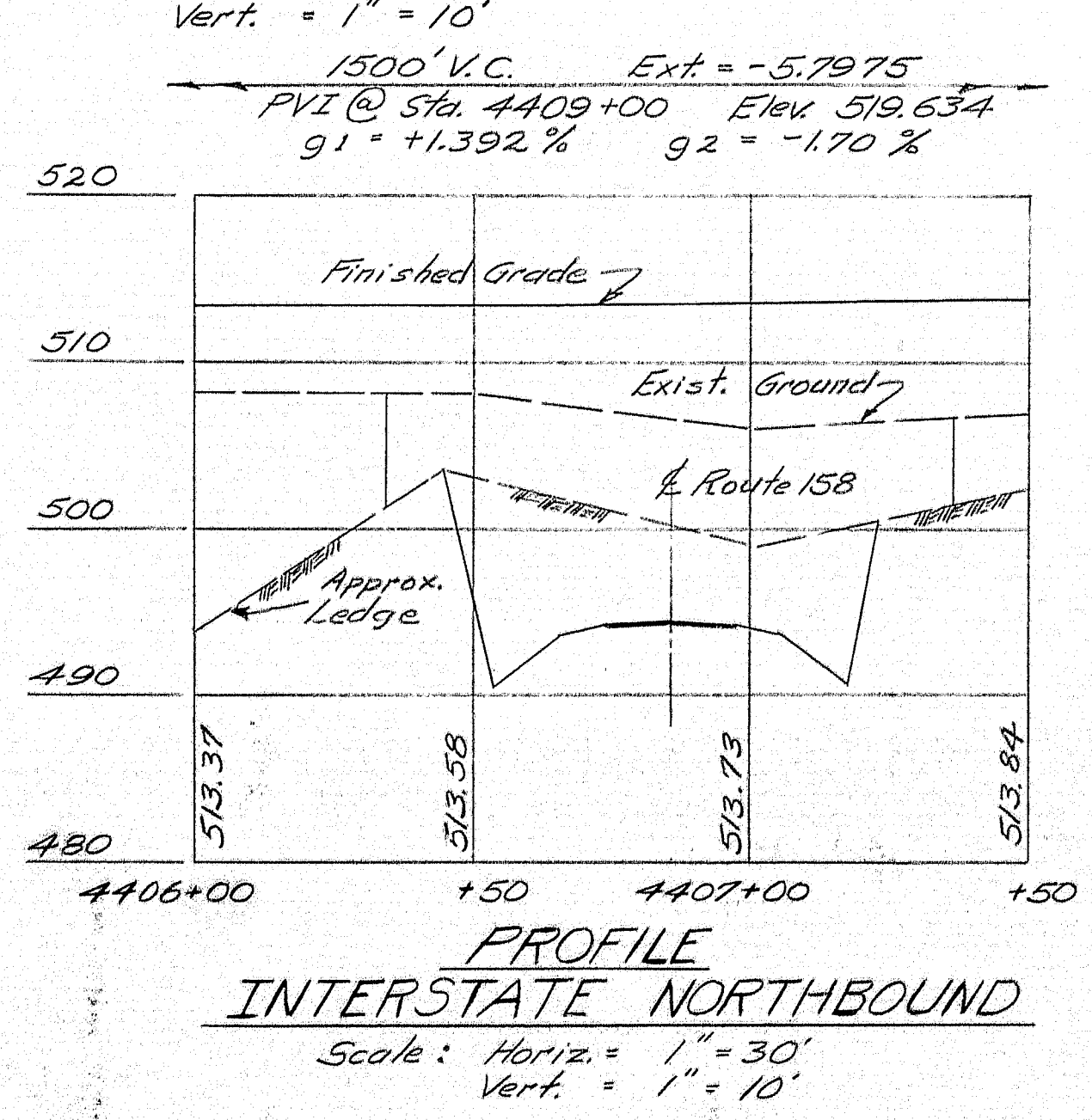
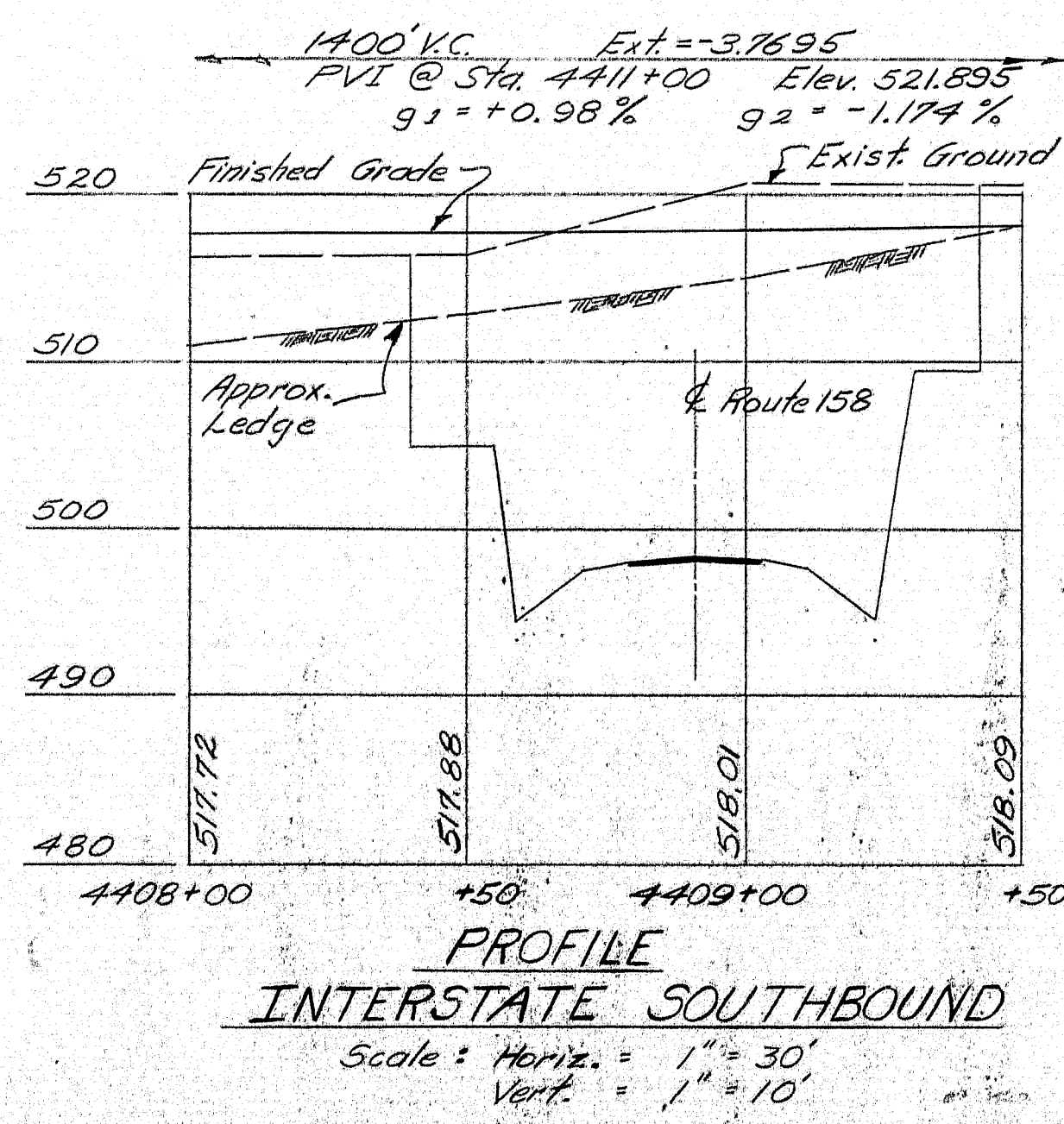
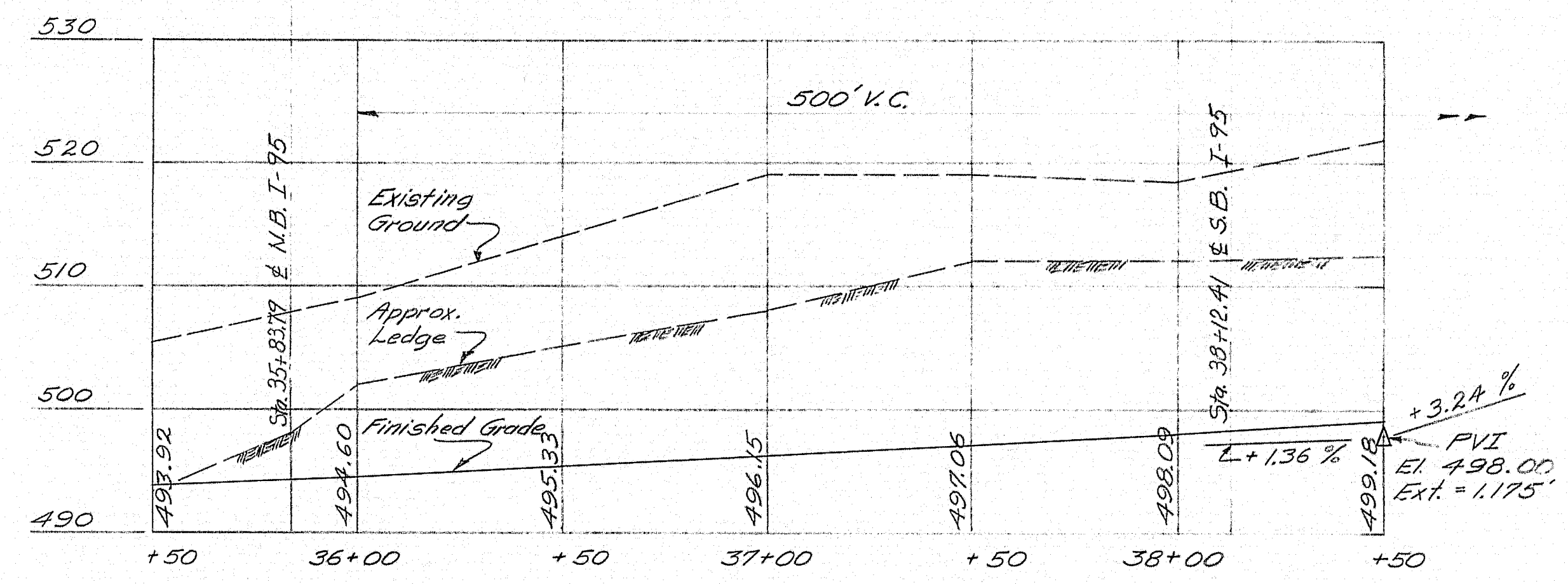
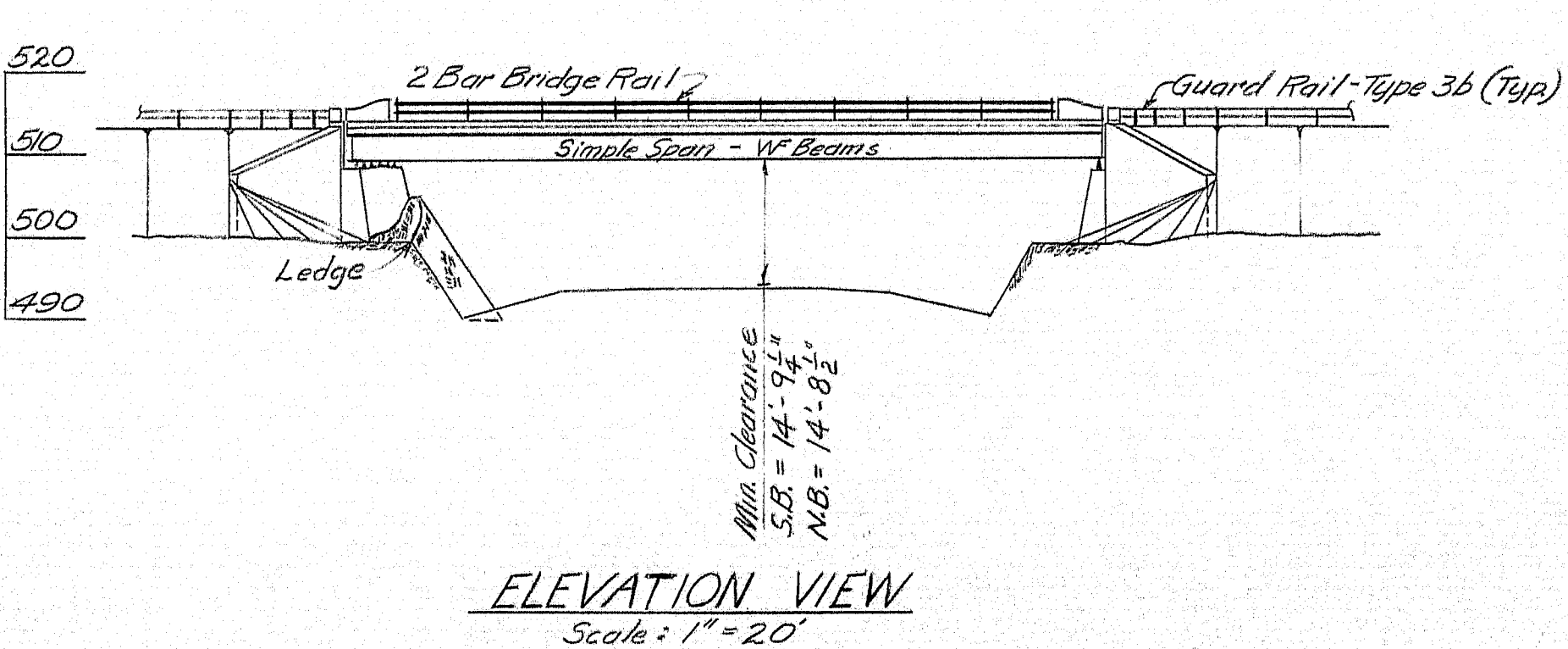
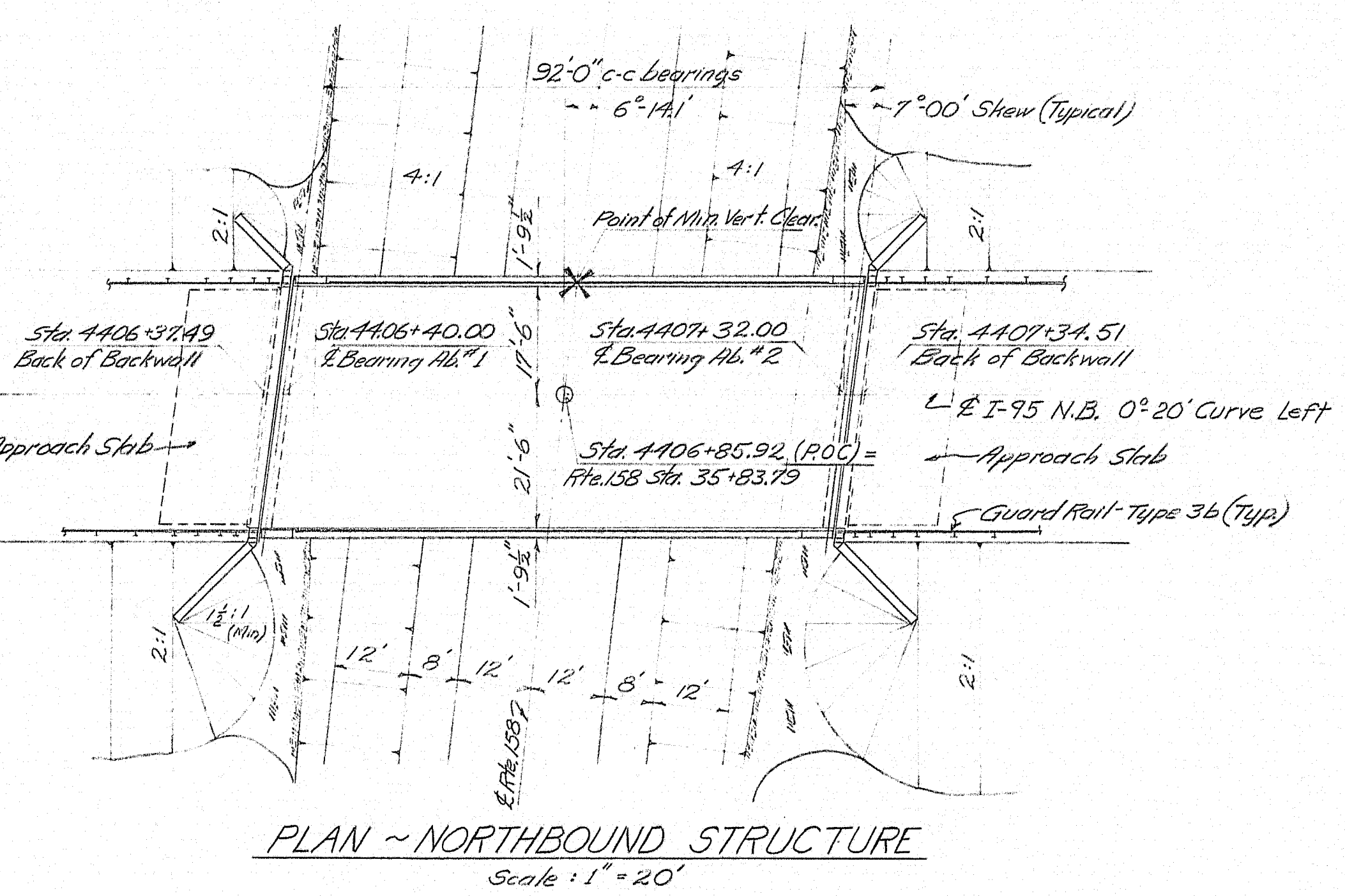
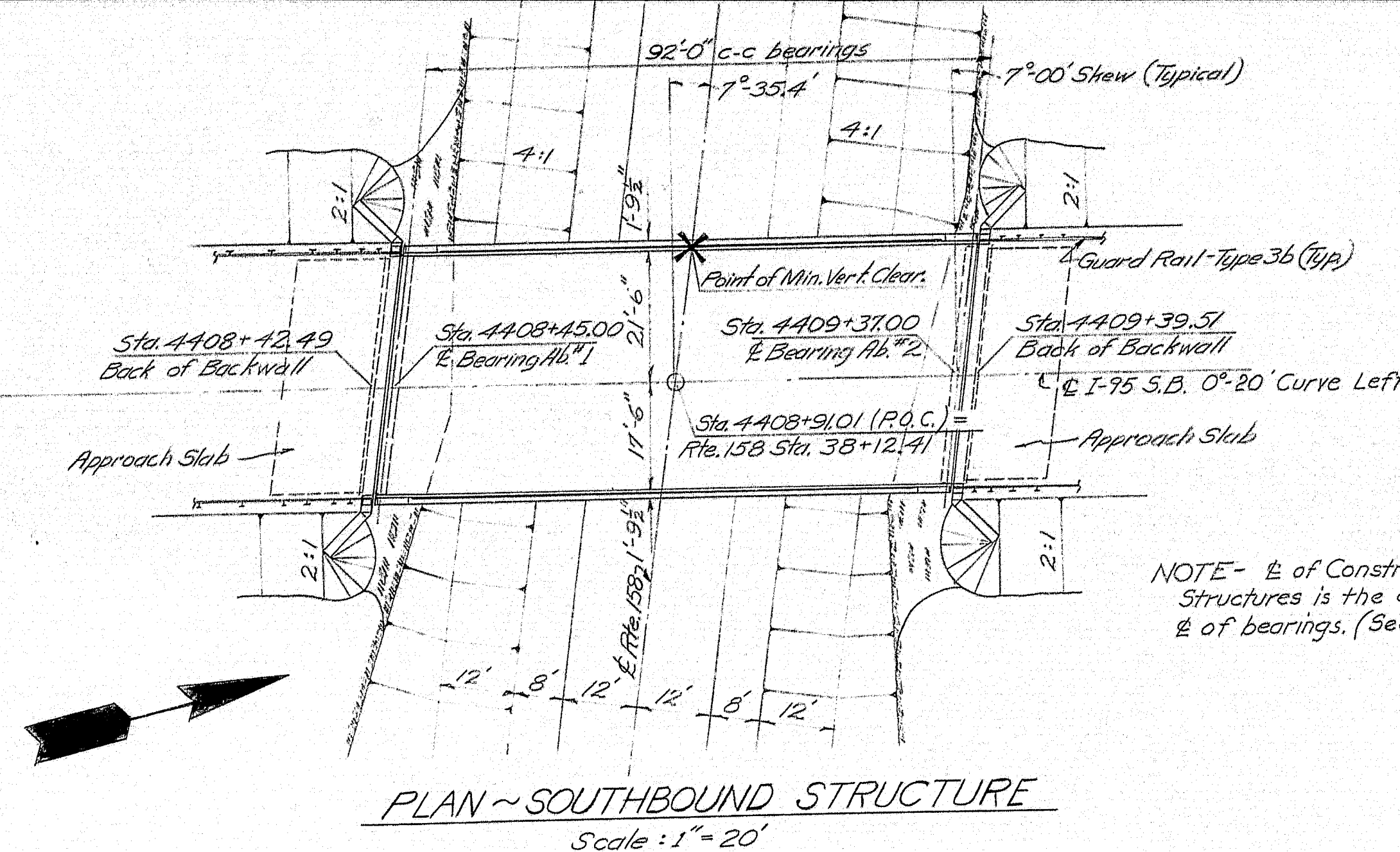
\* Bridge ~ Standard Details  
 BD 101-64 Bearing Pedestals  
 BD 104-66 Diaphragms, Annular Joint  
 Shear Connectors, Drain  
 BD 107-65 Steel Railings  
 BD 108-65 Aluminum Railings

\* For Bridge Standard Details see Benechete Road Plans

### ESTIMATE OF BRIDGE QUANTITIES N.B. & S.B.

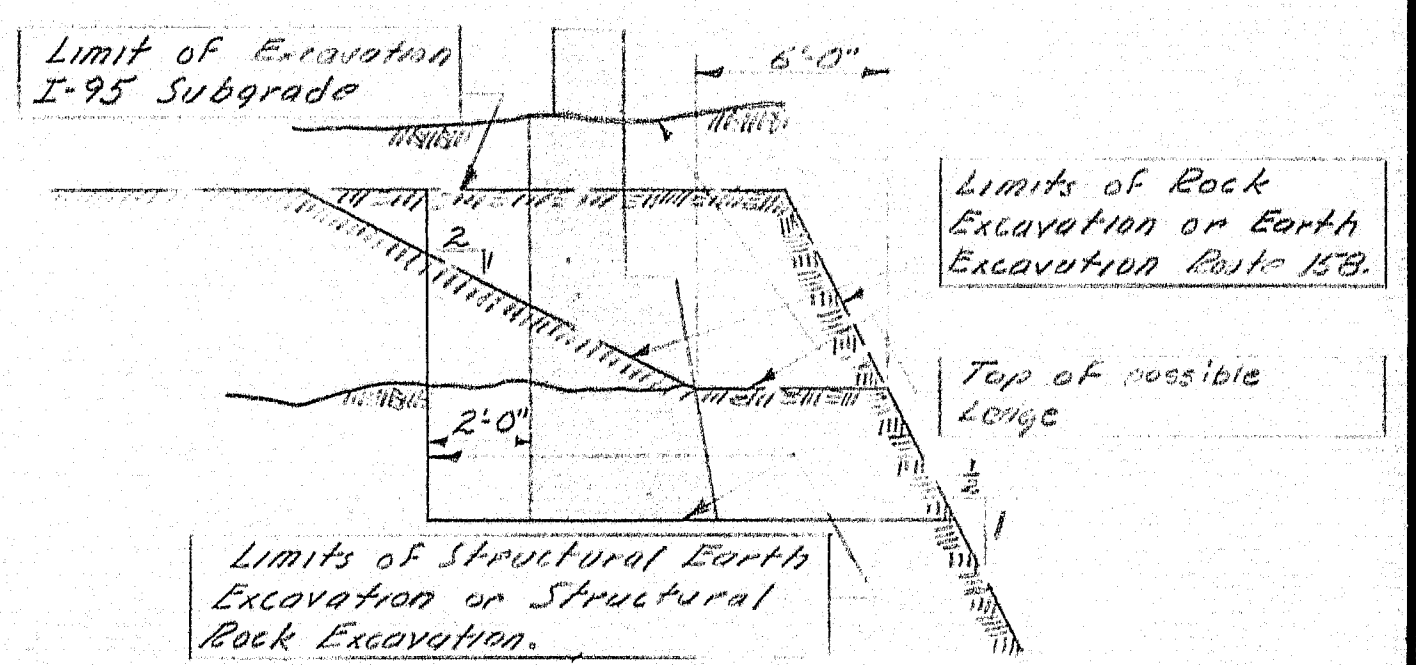
DESCRIPTION	UNIT	QUANTITY
Structural Earth Excavation Abutments & Retaining Walls	C.Y.	630
Structural Rock Excavation Abutments & Retaining Walls	C.Y.	185
Structural Concrete Abutments & Retaining Walls	C.Y.	290
Structural Concrete Roadway & Sidewalk Slabs on Steel Bridges (I-95 over Route 158)	L.S.	Lump Sum
Structural Concrete Approach Slabs (I-95 over Route 158)	L.S.	Lump Sum
Reinforcing Steel Fabricated & Delivered	Lbs.	70,850
Reinforcing Steel Piling	Lbs.	70,850
Structural Steel Fabricated & Delivered (I-95 over Route 158)	L.S.	Lump Sum
Structural Steel Erection (I-95 over Route 158)	L.S.	Lump Sum
Field Reinforcing Structural Steel (I-95 over Route 158)	L.S.	Lump Sum
Shear Connectors (I-95 over Route 158)	L.S.	Lump Sum
Bridge Railing	Lin. Ft.	330
Membrane Waterproofing	S.Y.	815
French Drains	C.Y.	140
Epoxy Resin Surface Sealant	S.Y.	225
Vertical Bridge Curbs, Type 1	Lin. Ft.	385
Hot Bituminous Pavement (Grading C-1 Crushed Stone)	Tons	90

Estimated Quantity of Structural Concrete Roadway and Sidewalk Slabs on Steel Bridges = 210 Cu. Yds.  
 Estimated Quantity of Structural Concrete Approach Slabs = 57 Cu. Yds.  
 Estimated Quantity of Structural Steel = 253,500 Lbs.



### SPECIFICATIONS

Design: A.M.S.H.O. Standard Specifications for Highway Bridges, 1961, with Interim Specifications thru 1964.  
 Contract: State of Maine, State Highway Commission, Standard Specifications for Highways and Bridges, Revision of June 1965.  
 Live Loading: HS20-44 as Modified for Interstate.  
 Concrete Classification: All concrete Class A.  
 Structural Steel Classification: All steel A36.  
 Allowable Stresses:  
 Concrete -  $f_c = 1200$  psi;  $n = 10$   
 Reinforcing Steel, intermediate grade -  $f_s = 20,000$  psi  
 Structural Steel, A441 = 27,000 psi (bulk) adjusted for fullness of material and fatigue.  
 A36 = 30,000 psi



TRAFFIC DATA

Year	ADT	Route 158	I-95
1966	1070	1490	
1966	1930	2150	
1966	232	258	
	14%	14%	
	60%	60%	
	50 M.P.H.	60 M.P.H.	

DESIGN - R.W.L.  
CHECK - H.L.D.

BRIDGE NO. SURVEY PLOT

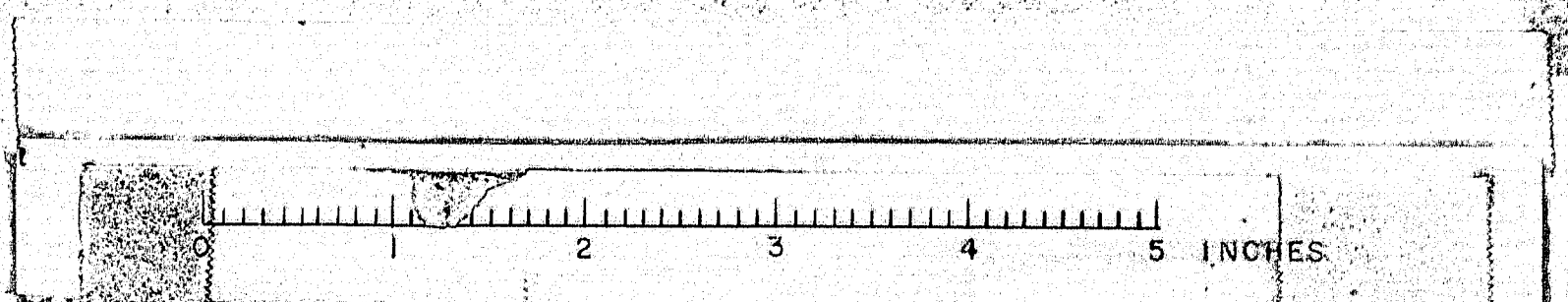
STATE HIGHWAY COMMISSION  
BRIDGE DIVISION

**INTERSTATE 95**  
OVER  
**ROUTE 158**  
IN THE TOWN OF  
**SHERMAN**  
**AROOSTOOK COUNTY**

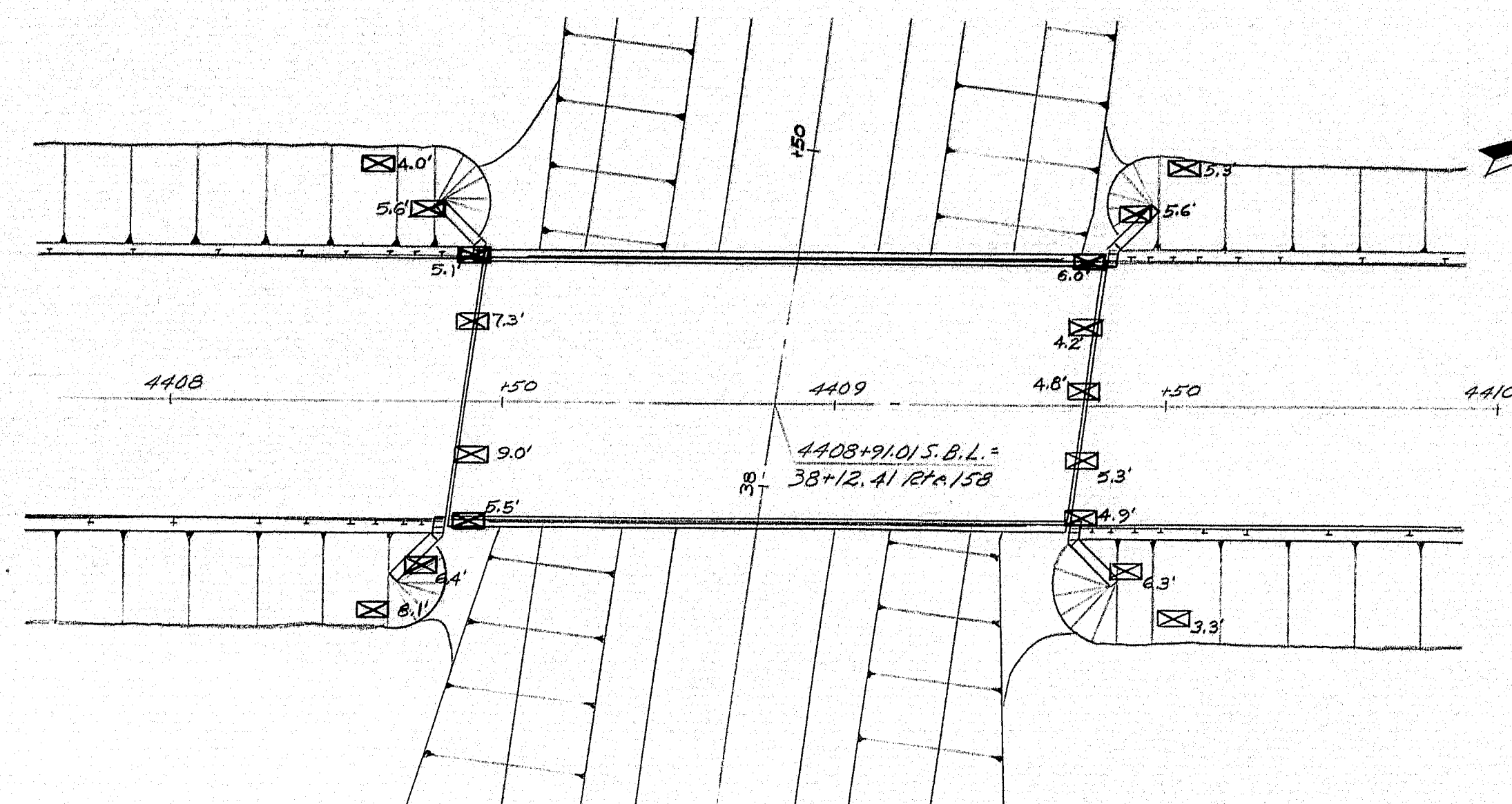
GENERAL PLAN & QUANTITIES

SHEET 1 OF 10 AUGUSTA, MAINE APRIL 1966

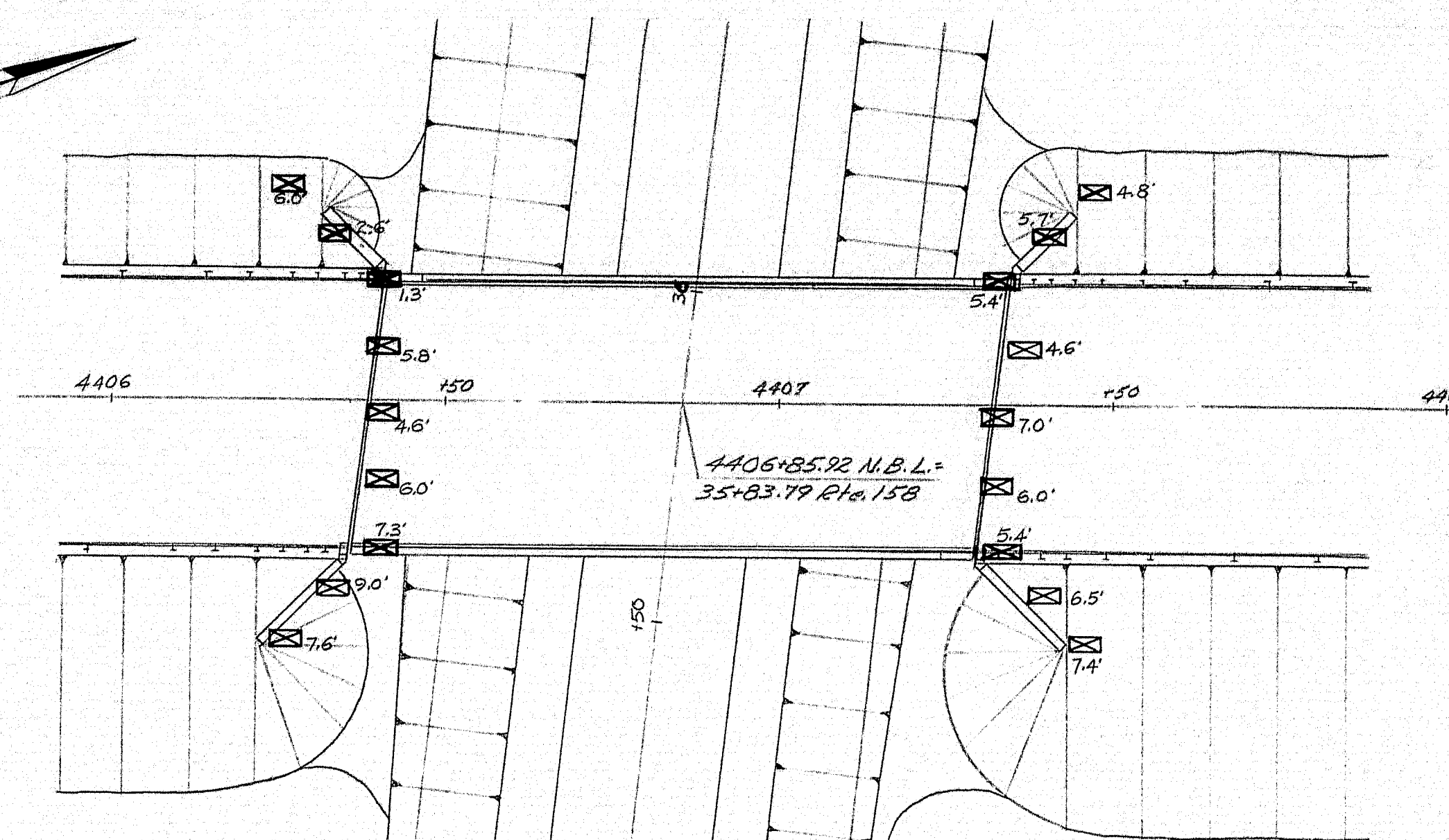
M-2516



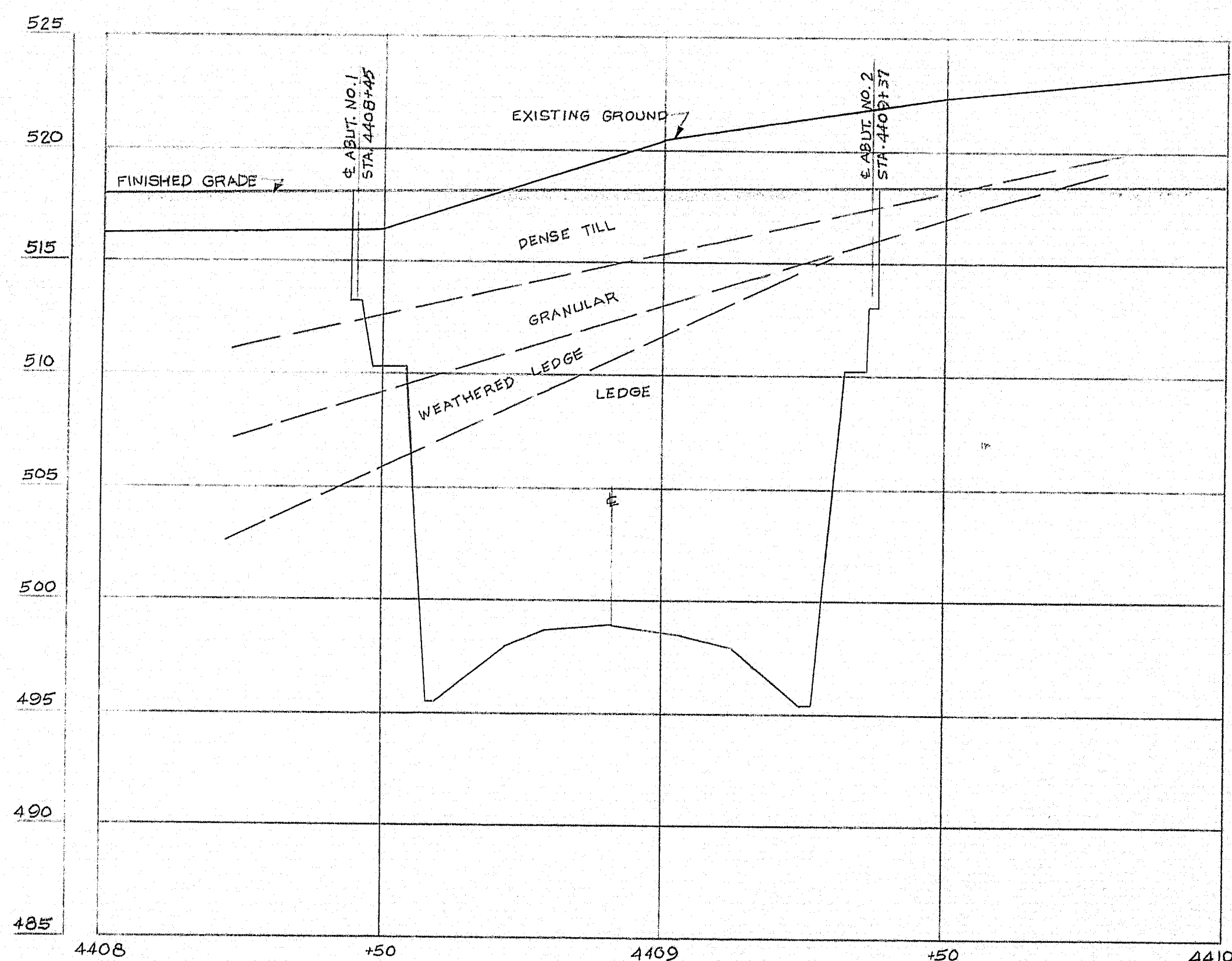




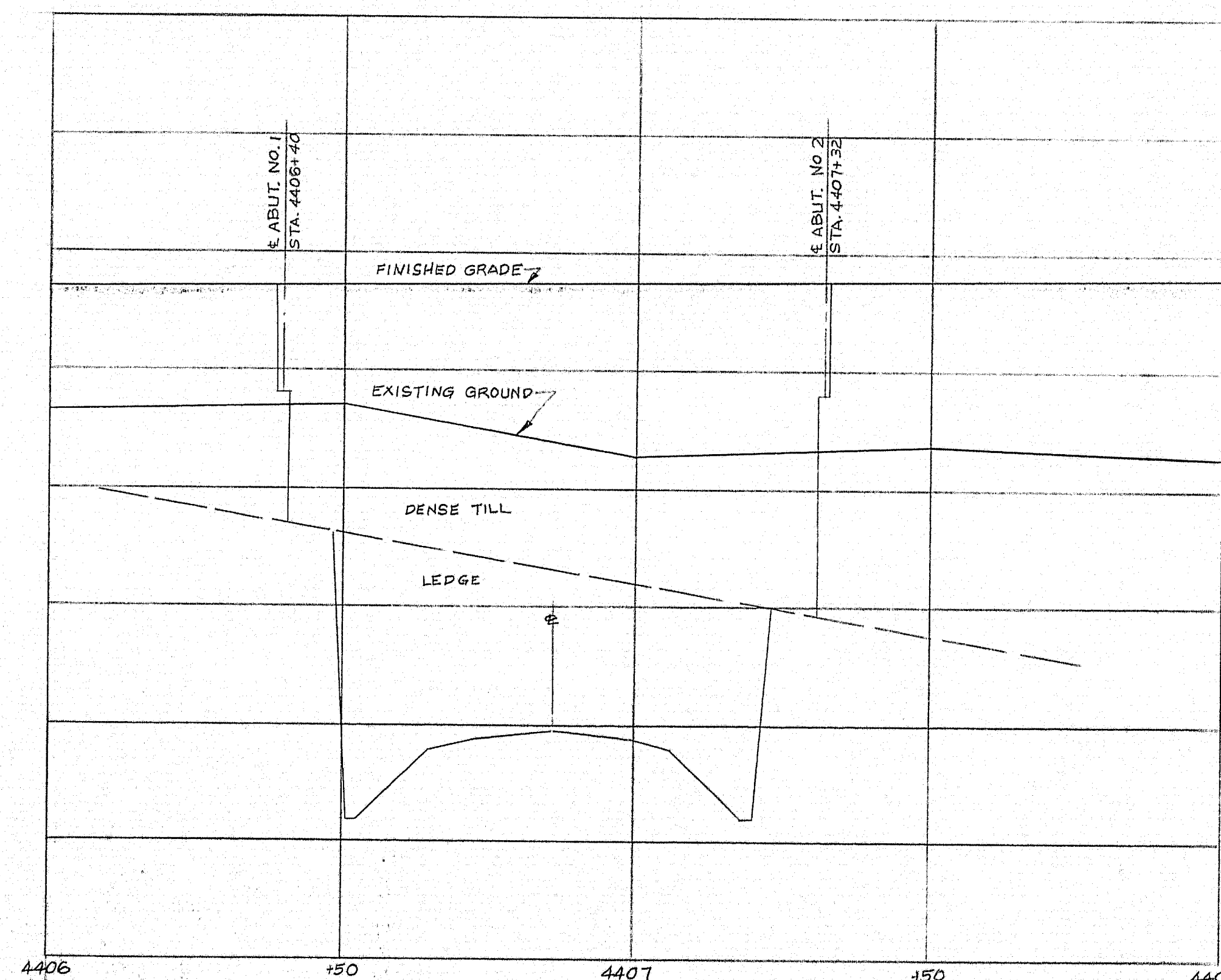
**SOUTHBOUND LANE PLAN**  
Scale: 1" = 20'



**NORTHBOUND LANE PLAN**  
Scale: 1" = 20'



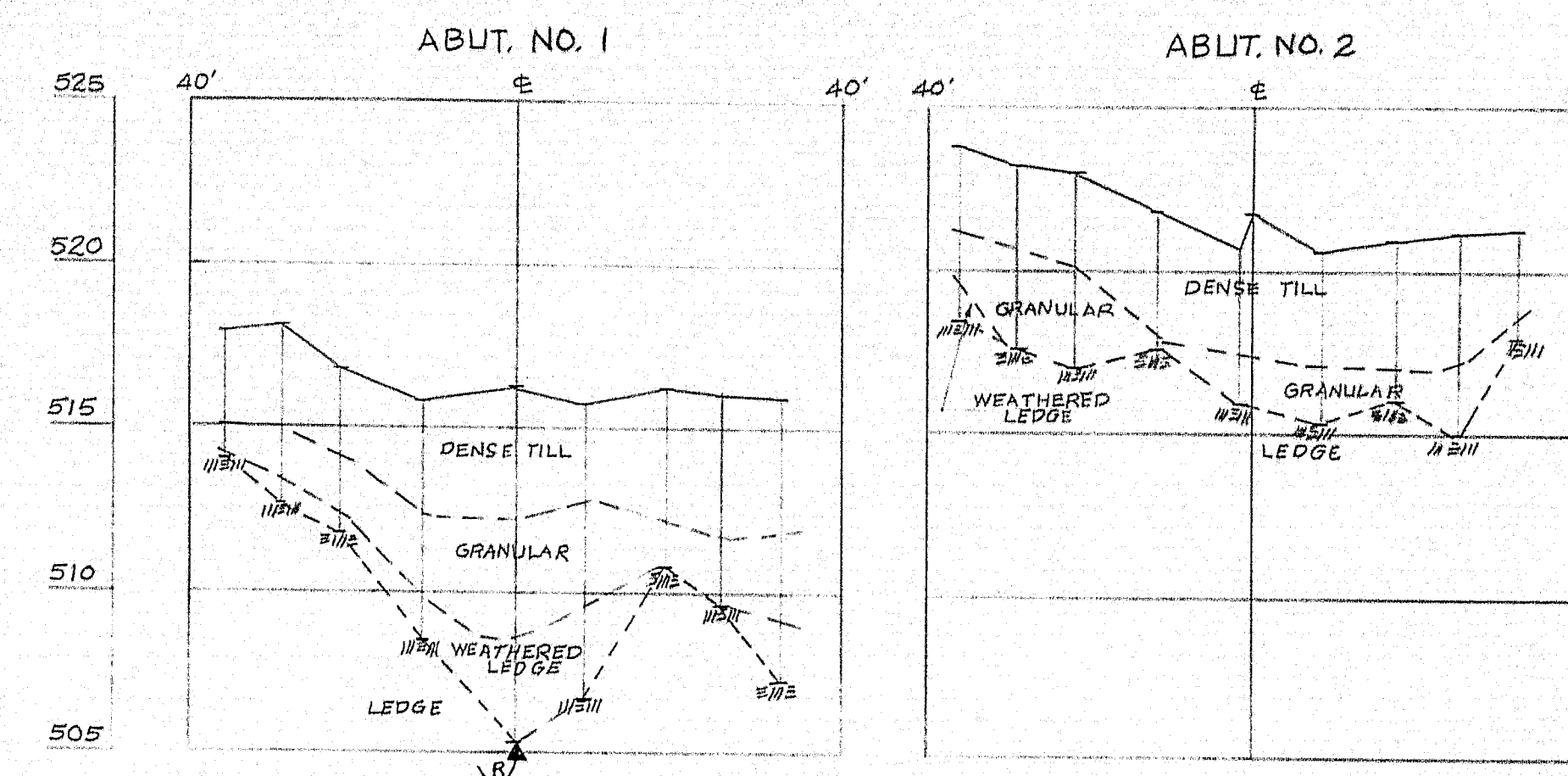
**SOUTHBOUND LANE PROFILE**  
Scales: 1" = 5' Vert.  
1" = 20' Horiz.



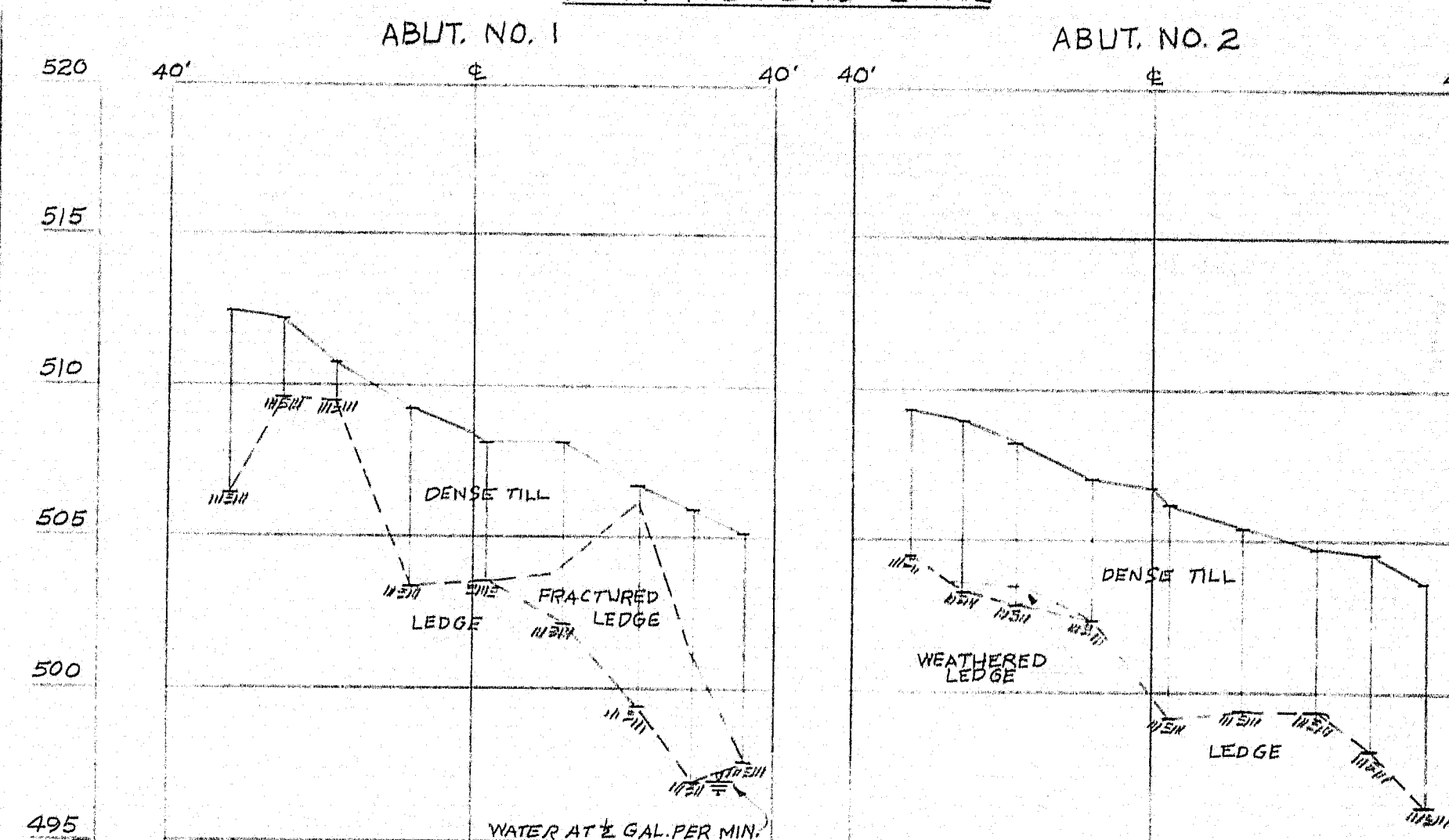
**NORTHBOUND LANE PROFILE**  
Scales: 1" = 5' Vert.  
1" = 20' Horiz.

**TRANSVERSE SECTIONS**

**SOUTHBOUND LANE**



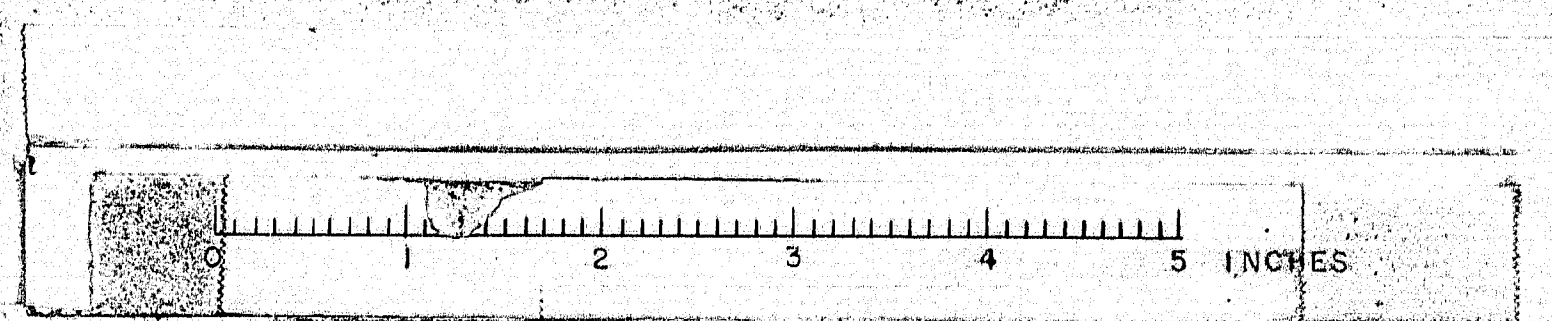
**NORTHBOUND LANE**



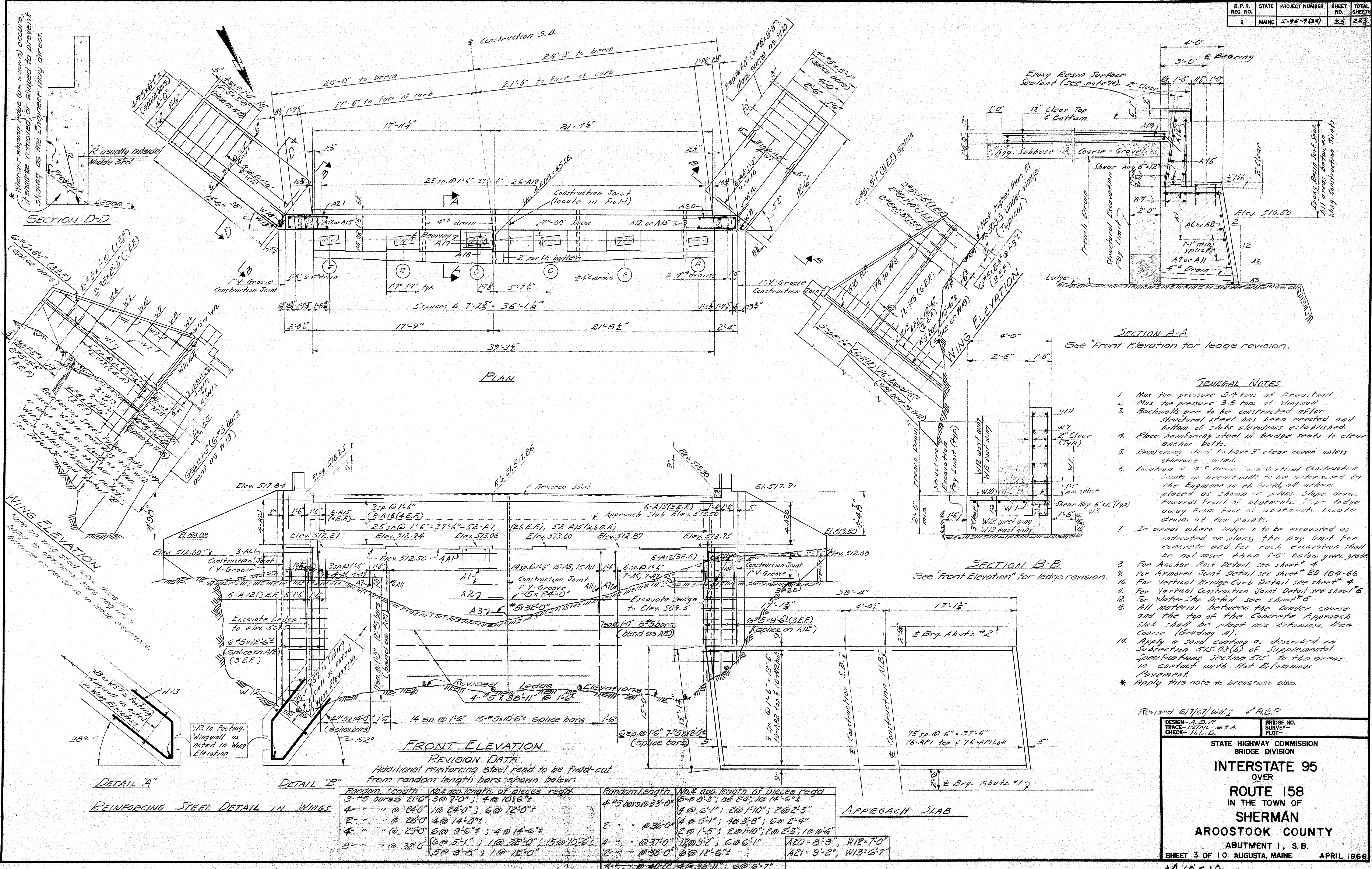
Note - This sheet is for foundation information only.  
Do not use for construction details.

DESIGN - TRACE - CHECK -	Soils Division	BRIDGE NO. SURVEY - PLOT -
STATE HIGHWAY COMMISSION BRIDGE DIVISION <b>INTERSTATE 95</b> OVER <b>ROUTE 158</b> IN THE TOWN OF <b>SHERMAN</b> <b>AROOSTOOK COUNTY</b> FOUNDATION SURVEY		
SHEET 2 OF 10 AUGUSTA, MAINE AUG. 1966		

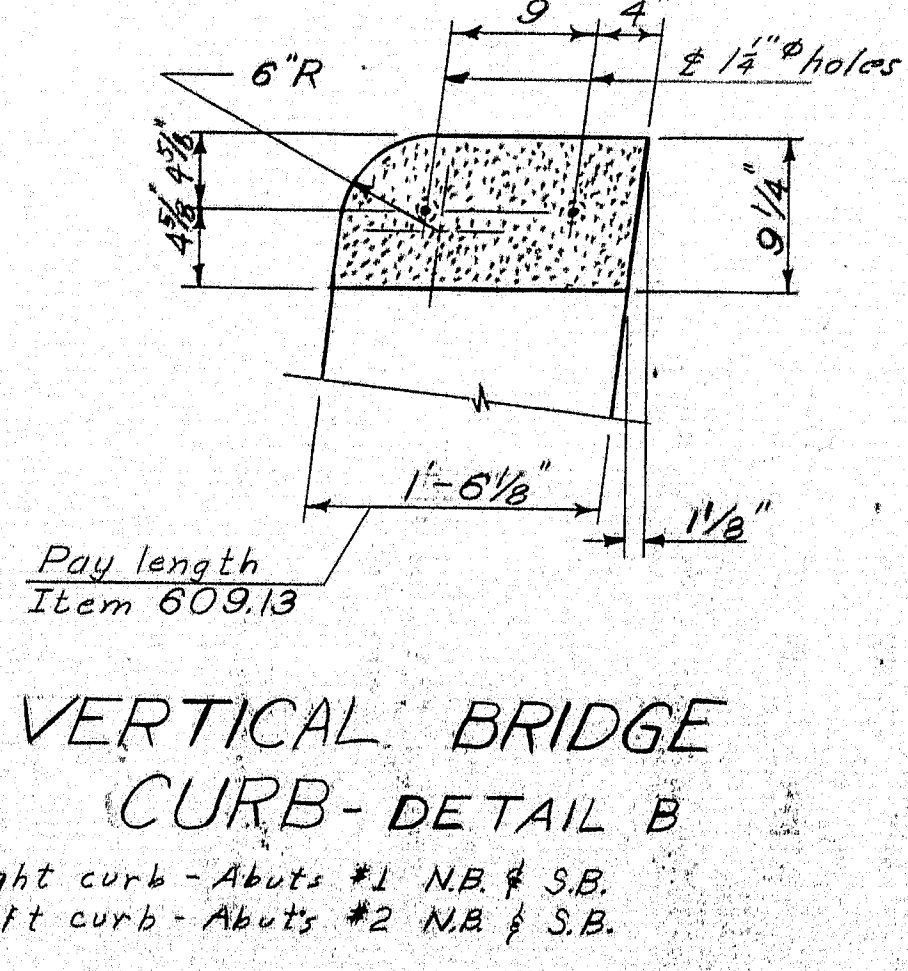
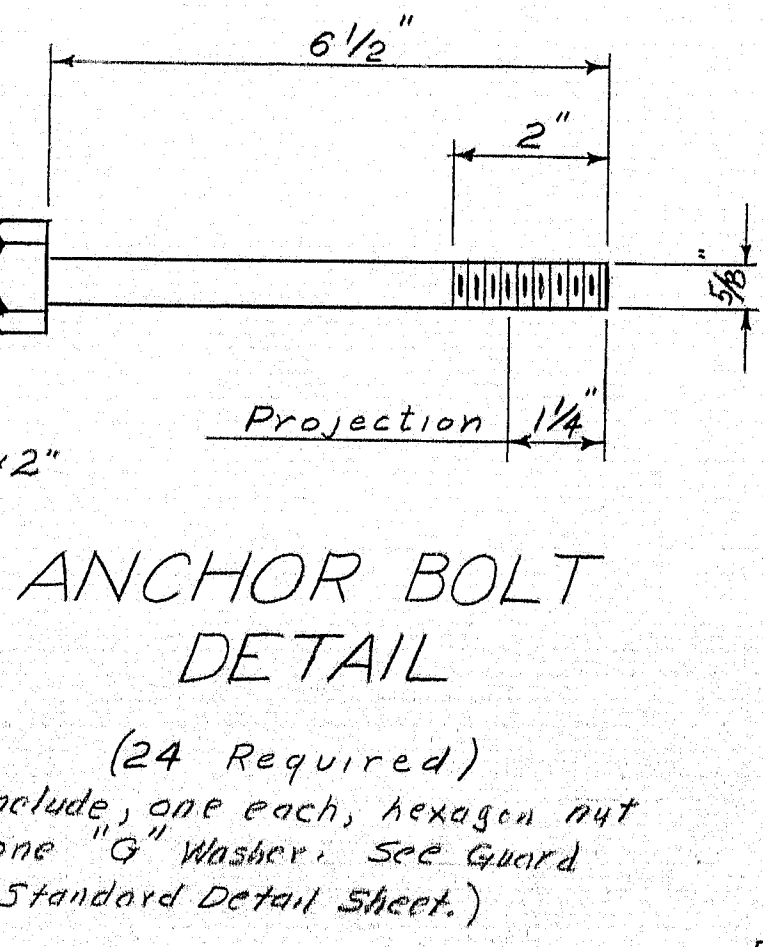
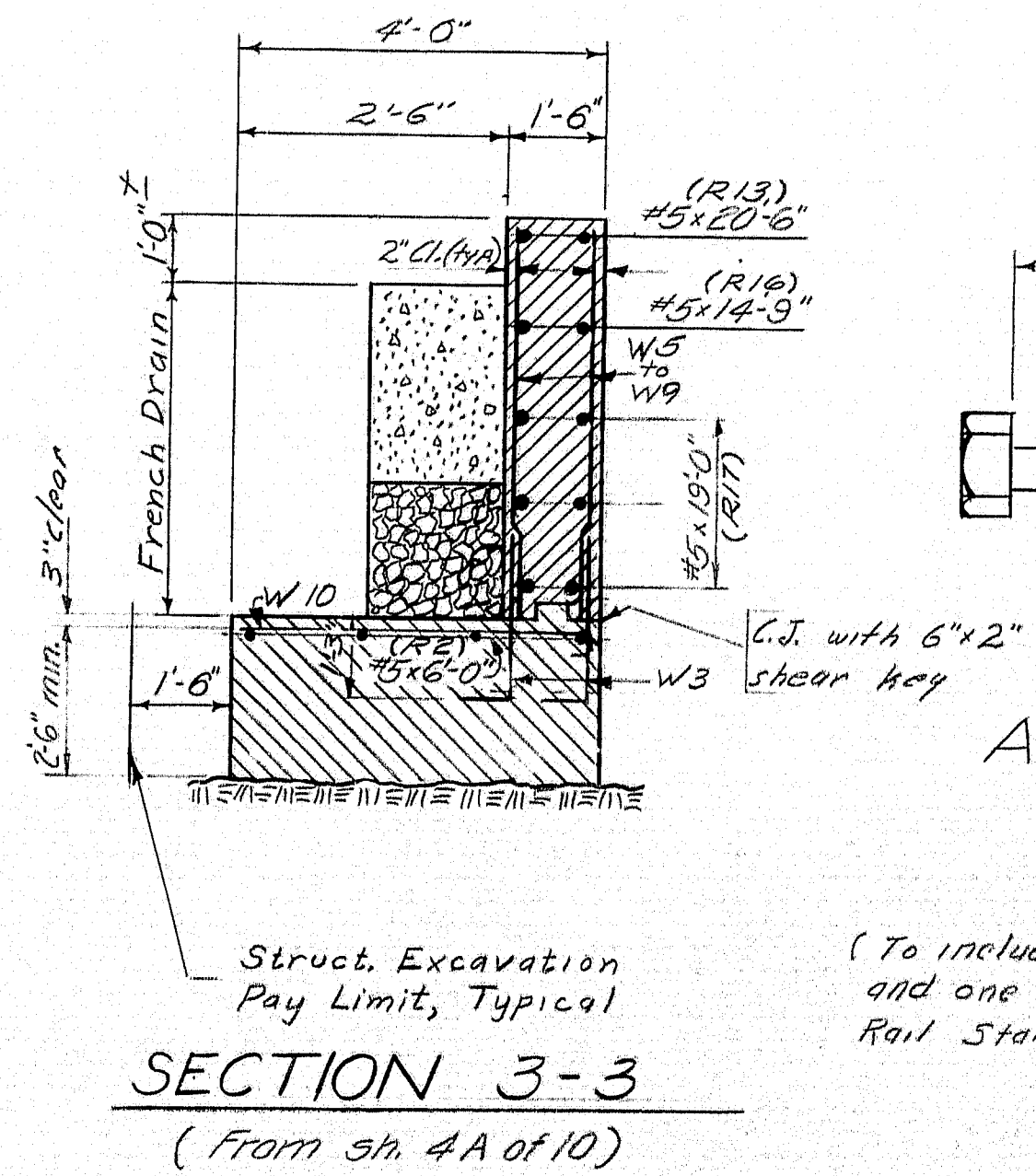
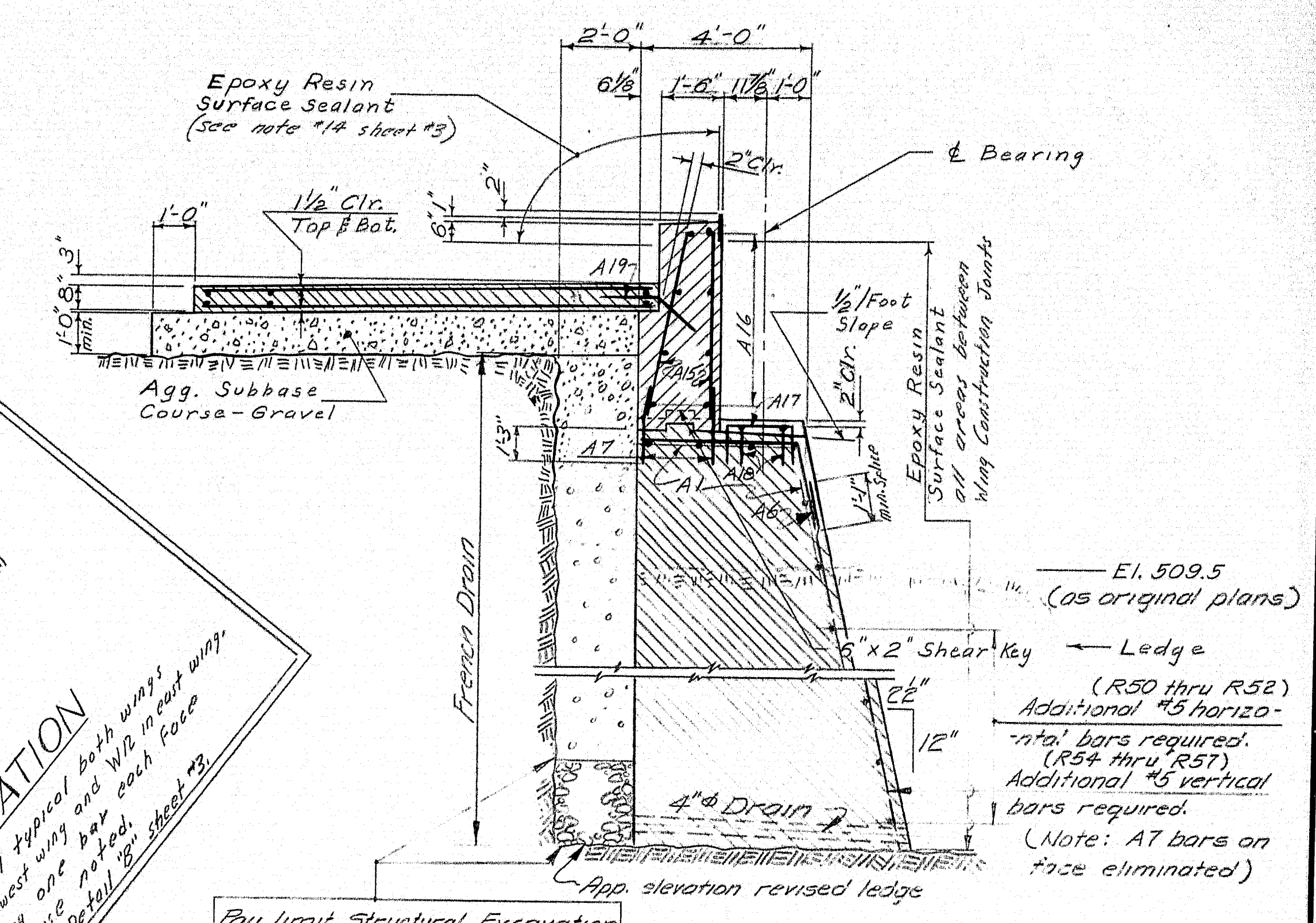
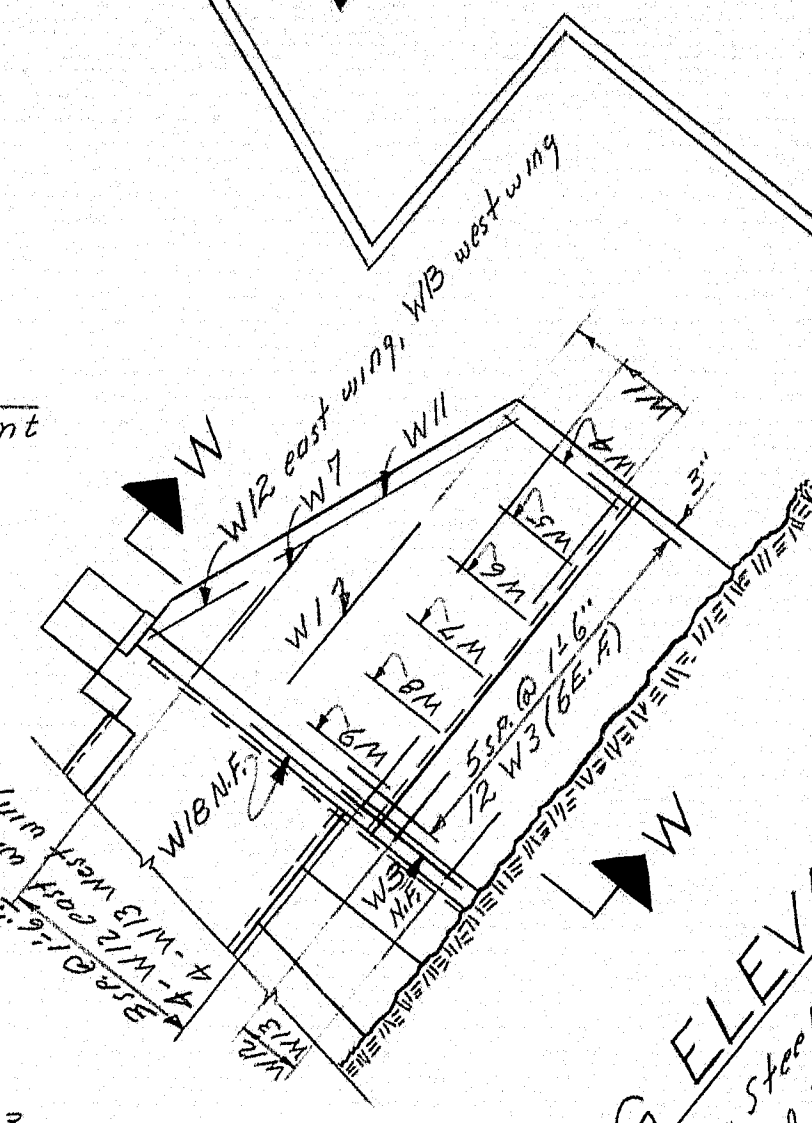
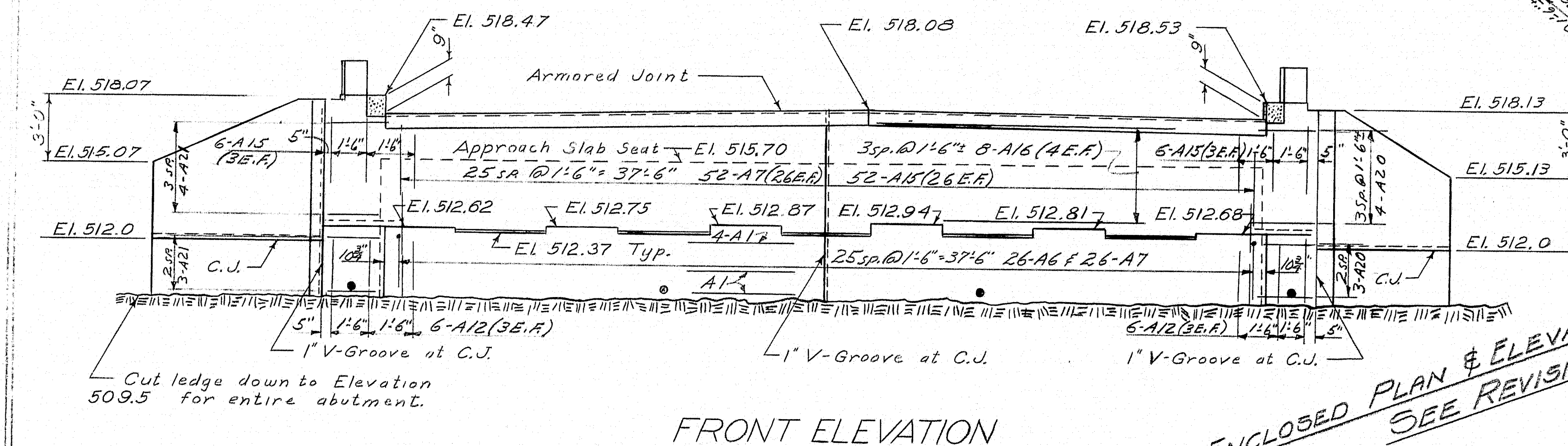
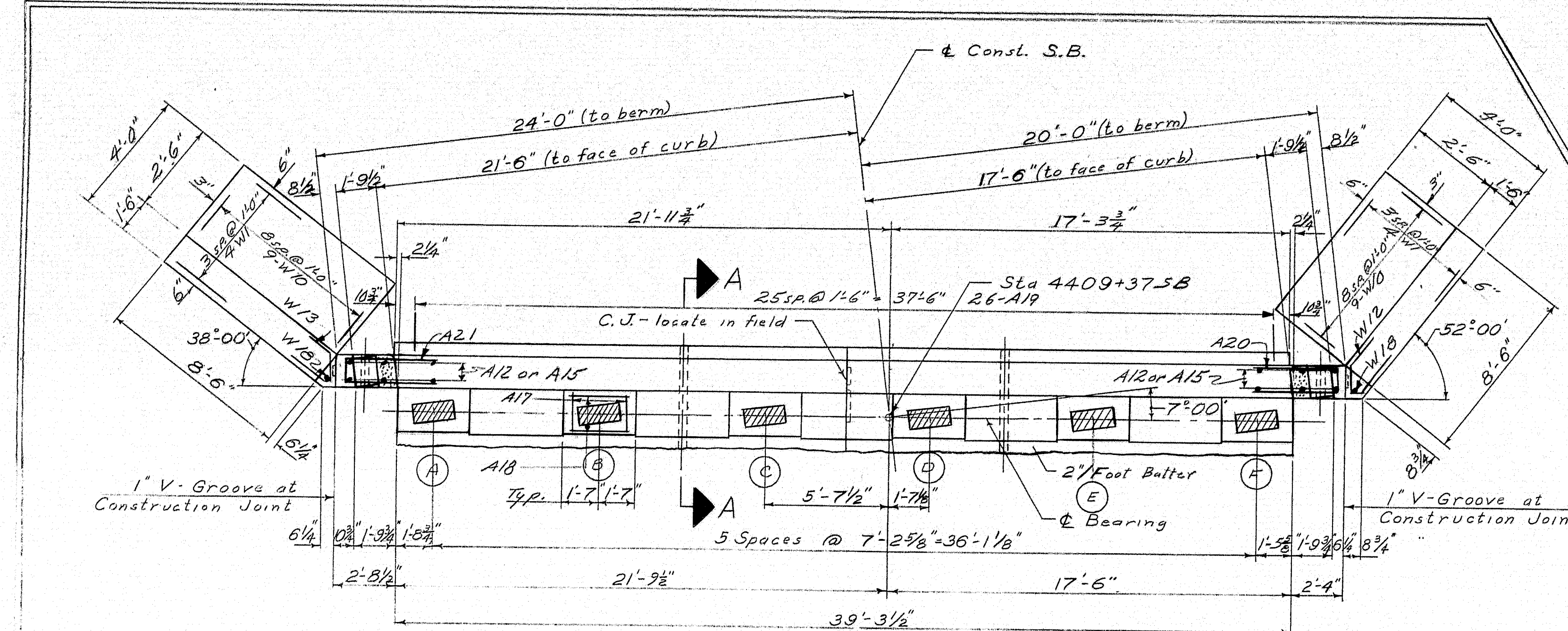
M-2517











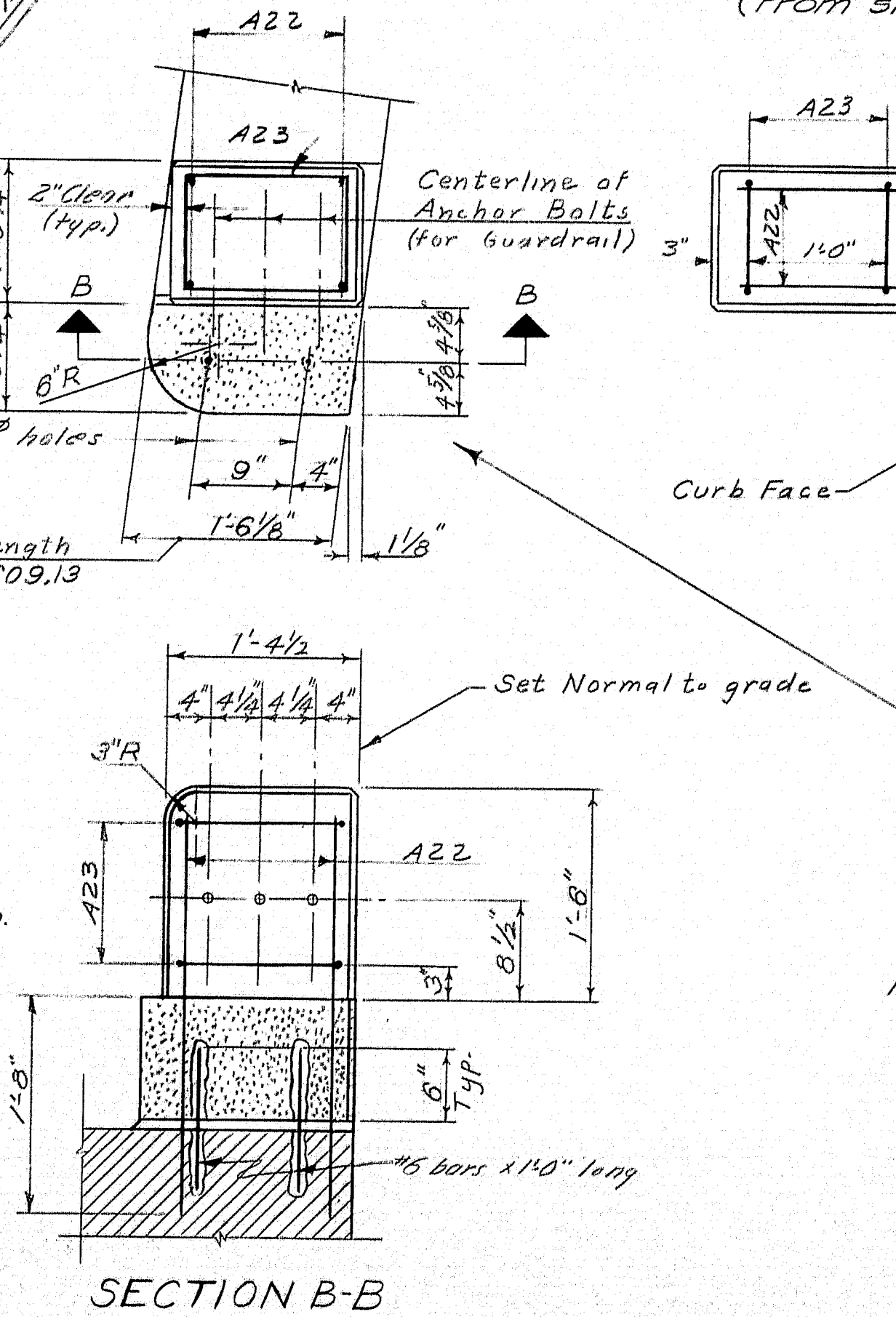
~NOTES~

Grout anchors into 1 1/4"  $\phi$  holes in stone prior to setting stone on backwall. Drill 1 1/4" holes in backwall to suit anchors.

Payment for drilling and grouting of anchors to be included in the price for Item 609.13, Vertical Bridge Curb, Type 1.

8 Corner stones required - 4 of each type shown. Concrete in Substructure posts to be paid for under Item 502.21, "Structural Concrete, Abutments & Retaining Walls."

See Sheet #3 for "General Abutment Notes."



CURB DETAIL A

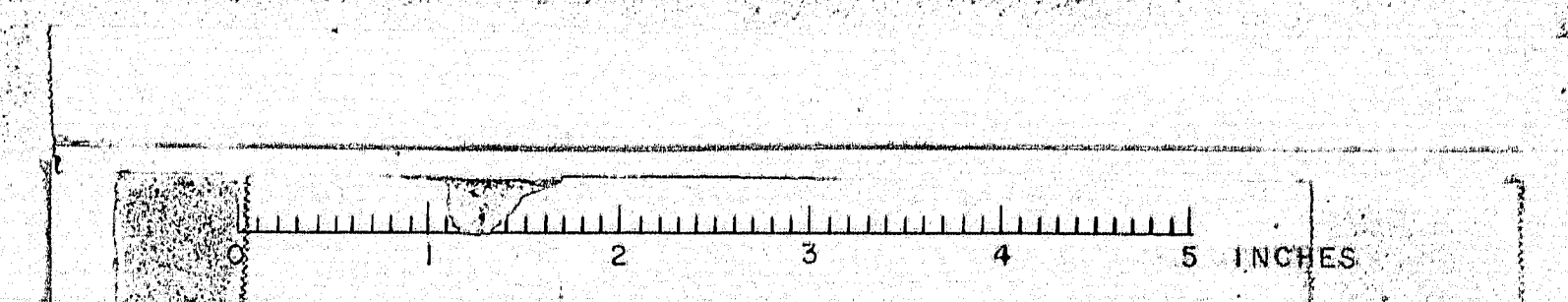
Left curb - Abuts. #1 N.B. & S.B.  
Right curb - Abuts. #2 N.B. & S.B.

Revised 7/6/67 by H.H. Checked A.B.P.

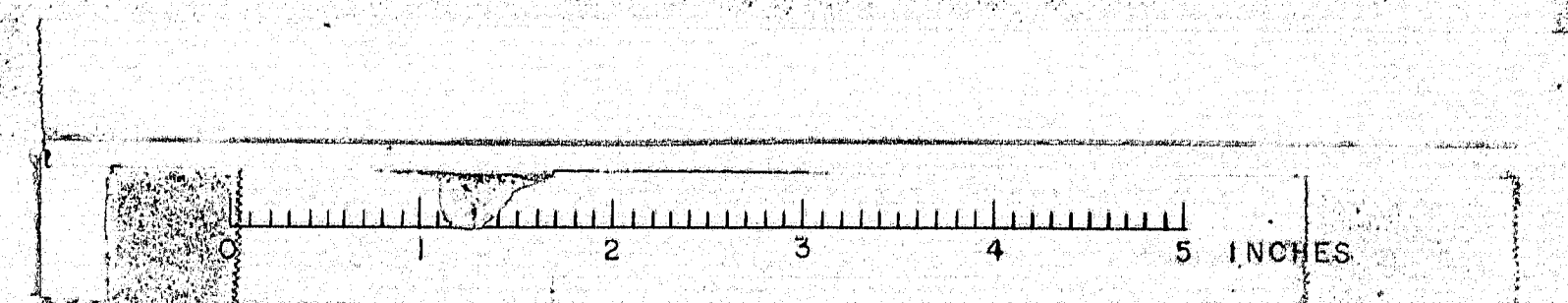
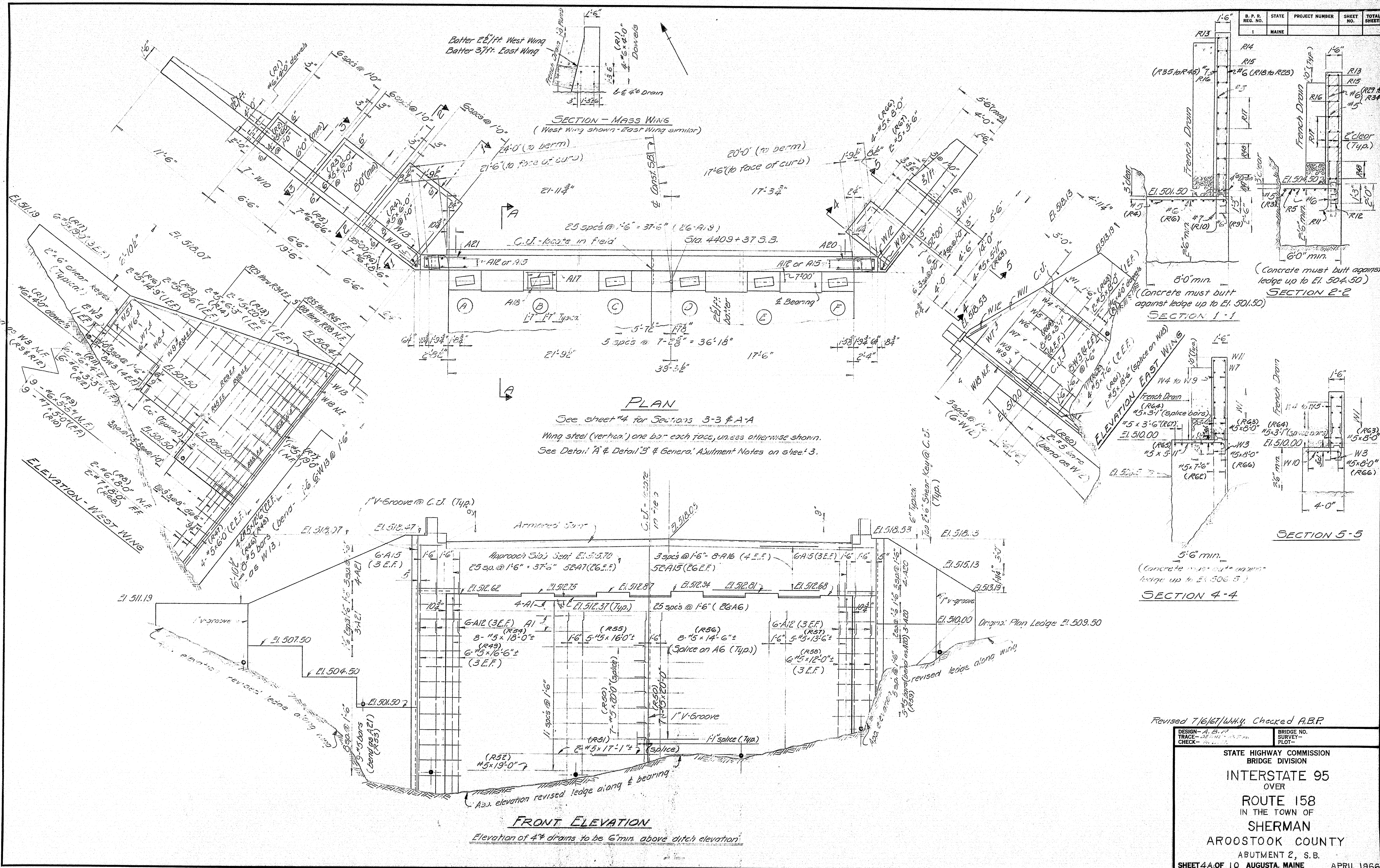
DESIGN - A.B.P.	BRIDGE NO. SURVEY - 609.13
TRACE - G.H.B.	PLOT -

STATE HIGHWAY COMMISSION  
BRIDGE DIVISION

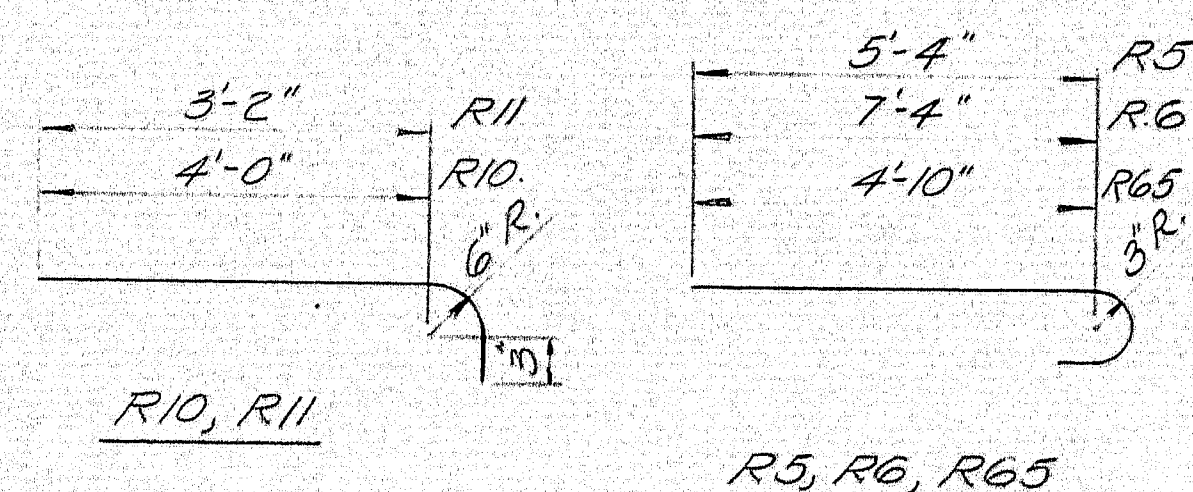
INTERSTATE 95  
OVER  
ROUTE 158  
IN THE TOWN OF  
SHERMAN  
AROOSTOOK COUNTY  
ABUTMENT 2, S.B.  
SHEET 4 OF 10 AUGUSTA, MAINE APRIL 1966











All dimensions to 1/2 bars.  
All reinforcing steel to be of intermediate grade -  $f_y = 20,000$  psi.

### BENT BARS

Mark	Size	Length	No.	Remarks
R5	#6	6'-6"	7	Footings West Wing
R6	#6	8'-6"	7	" " "
R9	#6	3'-3"	9	Footings West Wing - bend as W3 sh. 10
R10	#7	5'-0"	9	" " "
R11	#6	4'-2"	6	" " "
R12	#6	3'-3"	6	" " " - bend as W3 sh. 10
R46	#5	6'-7"	8	West Wing - bend as W13 sh. 10
R53	#5	9'-2"	9	Breastwall - bend as A21 sh. 10
R59	#5	8'-3"	5	" - bend as A20 "
R60	#5	7'-0"	2	East Wing - bend as W12 "
R65	#5	5'-11"	4	Footings East Wing

### STRAIGHT BARS

R1	#6	4'-0"	8	Dowels - Junction mass conc. wings
R2	#5	6'-0"	4	West Wing Footings
R3	#5	6'-0"	6	" " "
R4	#5	6'-0"	8	" " "
R7	#5	19'-0" ±	1	West Wing Wall & Footings
R8	#6	8'-0" ±	2	West Wing Footings
R13	#5	20'-6" ±	2	" " Wall (Horiz.)
R14	#5	6'-3" ±	2	" " "
R15	#5	10'-6" ±	2	" " "
R16	#5	14'-9" ±	2	" " "
R17	#5	19'-0"	6	" " " (Horiz.)
R18	#6	16'-4"	1	" " " (Vert.)
R19		16'-1"		" " "
R20		15'-11"		" " "
R21		15'-9"		" " "
R22		15'-7"		" " "
R23		15'-5"		" " "
R24		15'-2"		" " "
R25		14'-11"		" " "
R26		14'-8"		" " "
R27		14'-5"		" " "
R28	#6	14'-2"	1	" " "
R29	#6	10'-11"	2	" " "
R30		10'-7"		" " "
R31		10'-3"		" " "
R32		9'-10"		" " "
R33		9'-5"		" " "
R34	#6	9'-0"	2	" " "
R35	#7	16'-4"	1	" " "
R36		16'-1"		" " "
R37		15'-11"		" " "
R38	#7	15'-9"	1	" " " (Vert.)

### STRAIGHT BARS CONT'D

Mark	Size	Length	No.	Remarks
R39	#7	15'-7"	1	West Wing Wall (Vert.)
R40		15'-5"		" " "
R41		15'-2"		" " "
R42		14'-11"		" " "
R43		14'-8"		" " "
R44		14'-5"		" " "
R45	#7	14'-2"	1	" " "
R47	#5	6'-0"	4	West Wing Wall (Horiz.)
R48	#5	12'-6"	4	" " "
R49	#5	16'-6" ±	6	Breastwall (West end)
R50	#5	20'-0"	14	Breastwall (Horiz.)
R51	#5	17'-1" ±	2	" " "
R52	#5	19'-0" ±	1	" " "
R54	#5	18'-0" ±	8	Breastwall (Vert.)
R55	#5	16'-0" ±	5	" " "
R56	#5	14'-6" ±	8	" " "
R57	#5	13'-6" ±	3	" " "
R58	#5	12'-0" ±	6	" " (East end)
R61	#5	13'-6" ±	1	East Wing Wall & Footings
R62	#5	7'-6" ±	4	" " "
R63	#5	8'-0"	2	East Wing Wall (Horiz.)
R64	#5	3'-1"	8	" (splice bars)
R66	#5	8'-0"	4	East Wing Footings
R67	#5	3'-6"	2	" " "
R68	#7	8'-0"	2	West Wing Footings

### SCHEDULE FOR ORDERING RANDOM LENGTHS (Bars to be field-cut)

No.	Size	Random Lengths	Bars Produced
3	#5	21'-0"	2(R67+R60), 2(R51+R64)
20		20'-0"	6(R49+R64), (14-R50)
9		18'-0"	(8-R54), 1(R53+R59)
8		16'-0"	(5-R55), (2-R63), (4-R66)
10		5'-0"	(8-R56), 4(R62)
6		14'-0"	(5-R57), (1-R61)
8	#5	12'-0"	(6-R53), (4-R65)
1	#6	16'-0"	(4-R1)
5	#5	19'-0"	(8-R53), (1-R52)
2	#5	17'-0"	(4-R59)
3	#5	21'-0"	(2-R15), (2-R13)
7		18'-0"	(6-R17), (1-R7)
2		18'-0"	(6-R3)
2		15'-0"	(2-R16)
4		14'-0"	(8-R46)
8	#5	12'-0"	(4-R47), (8-R4), (4-R2)
2	#6	22'-0"	(2-R29), (2-R30)
1	#6	21'-0"	(2-R31)
2		20'-0"	(6-R12), (2-R32)
1		19'-0"	(2-R33)
1		17'-0"	(1-R18)
8		16'-0"	(4-R1), (2-R8), (1-R19), (1-R20), (1-R21), (1-R22), (1-R23), (1-R24)
11		15'-0"	(7-R5+7-R6), (1-R25), (1-R26), (1-R27), (1-R28)
2	#6	13'-0"	(6-R11)
2	#7	17'-0"	(1-R35), (1-R36)
6		16'-0"	(1-R40), (1-R41), (1-R37), (1-R38), (1-R39), (2-R68)
7	#7	15'-0"	(1-R42), (1-R43), (1-R44), (1-R45), (9-R10)
5	#5	13'-0"	(2-R14), (4-R48)
1	#6	18'-0"	(2-R34)
3	#6	10'-0"	(9-R9)

Bars for Breastwall & East Wing

Bars for West Wing

ADDITIONAL REINFORCING STEEL REQ'D DUE TO REVISION OF ABUT 2 S.B.

DESIGN - M.H.Y.  
TRACE - M.H.Y.  
CHECK - R.B.P.

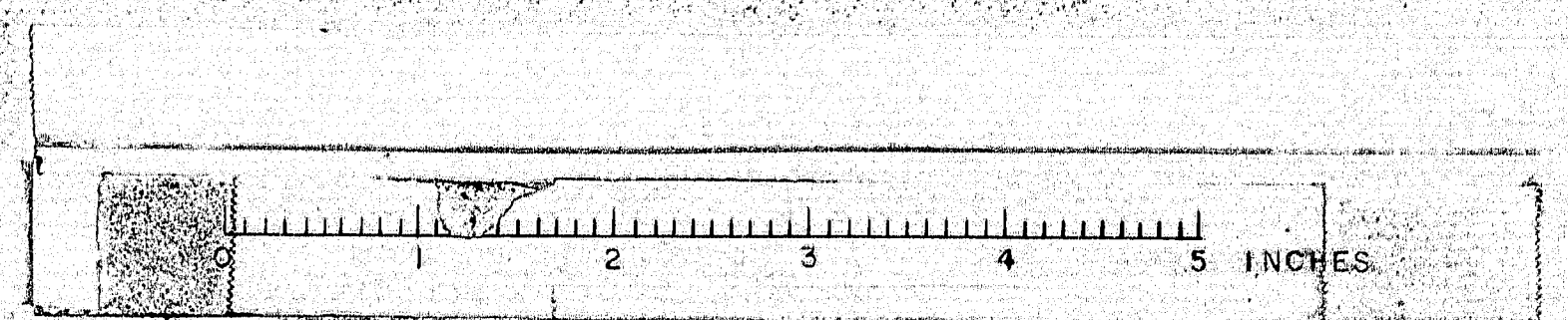
BRIDGE NO. 95  
SURVEY -  
PLOT -

STATE HIGHWAY COMMISSION  
BRIDGE DIVISION

INTERSTATE 95  
OVER  
ROUTE 158  
IN THE TOWN OF  
SHERMAN  
ARROOSTOOK COUNTY

ADDITIONAL REINFORCING STEEL ABUT 2 S.B.  
SHEET 48 OF 10 AUGUSTA, MAINE JULY 1967

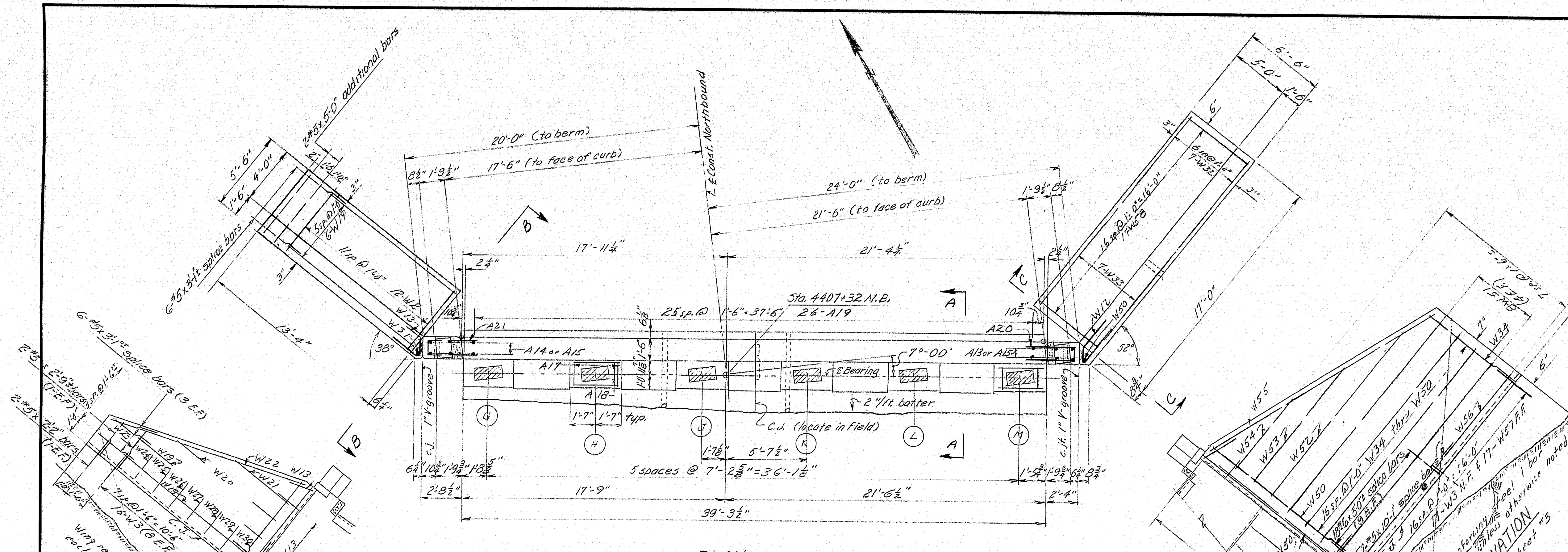
M-2521



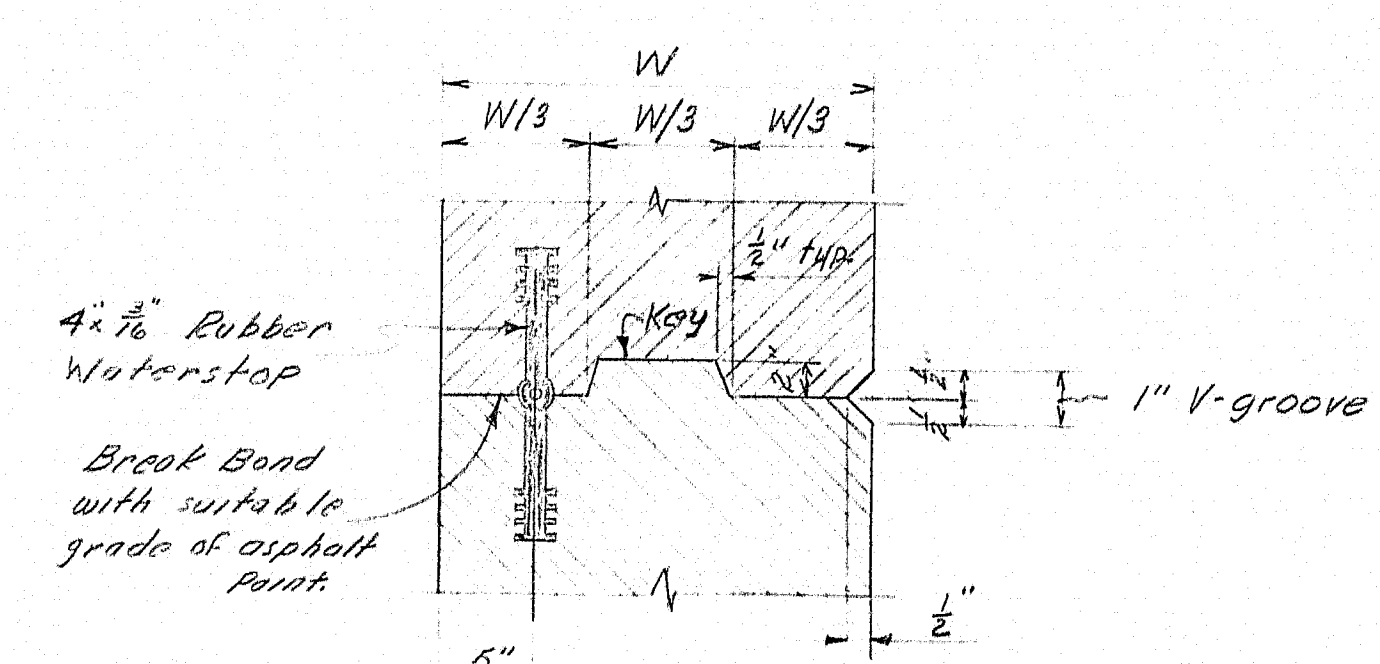






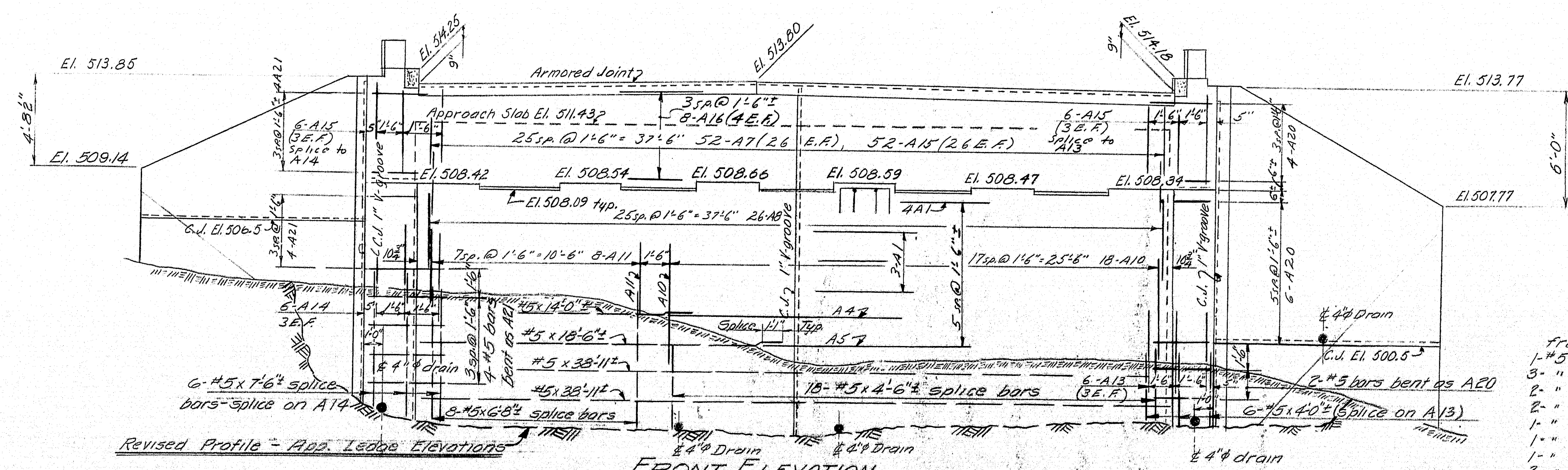


PLAN

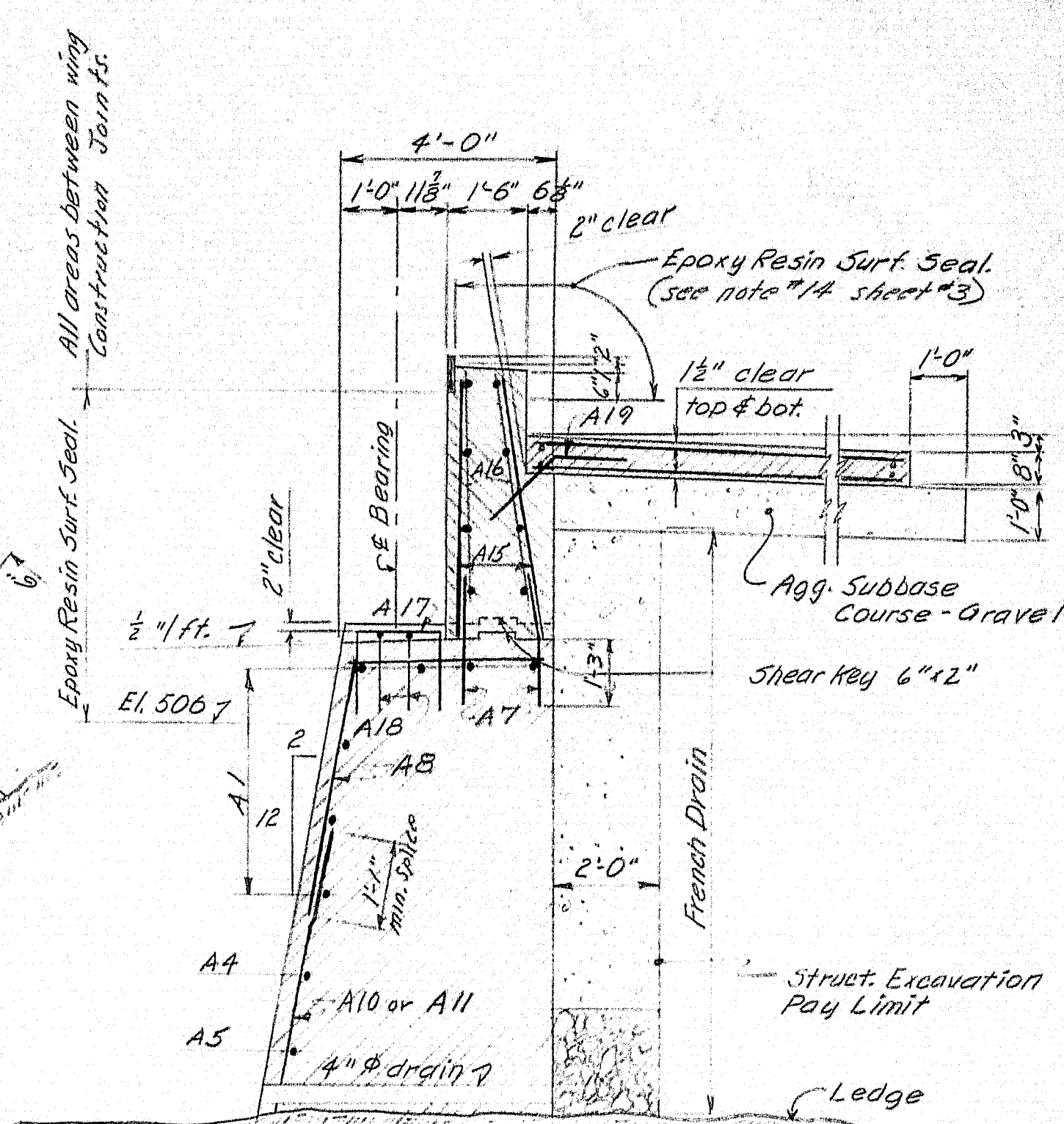


DETAIL - VERTICAL CONSTR. JOINT

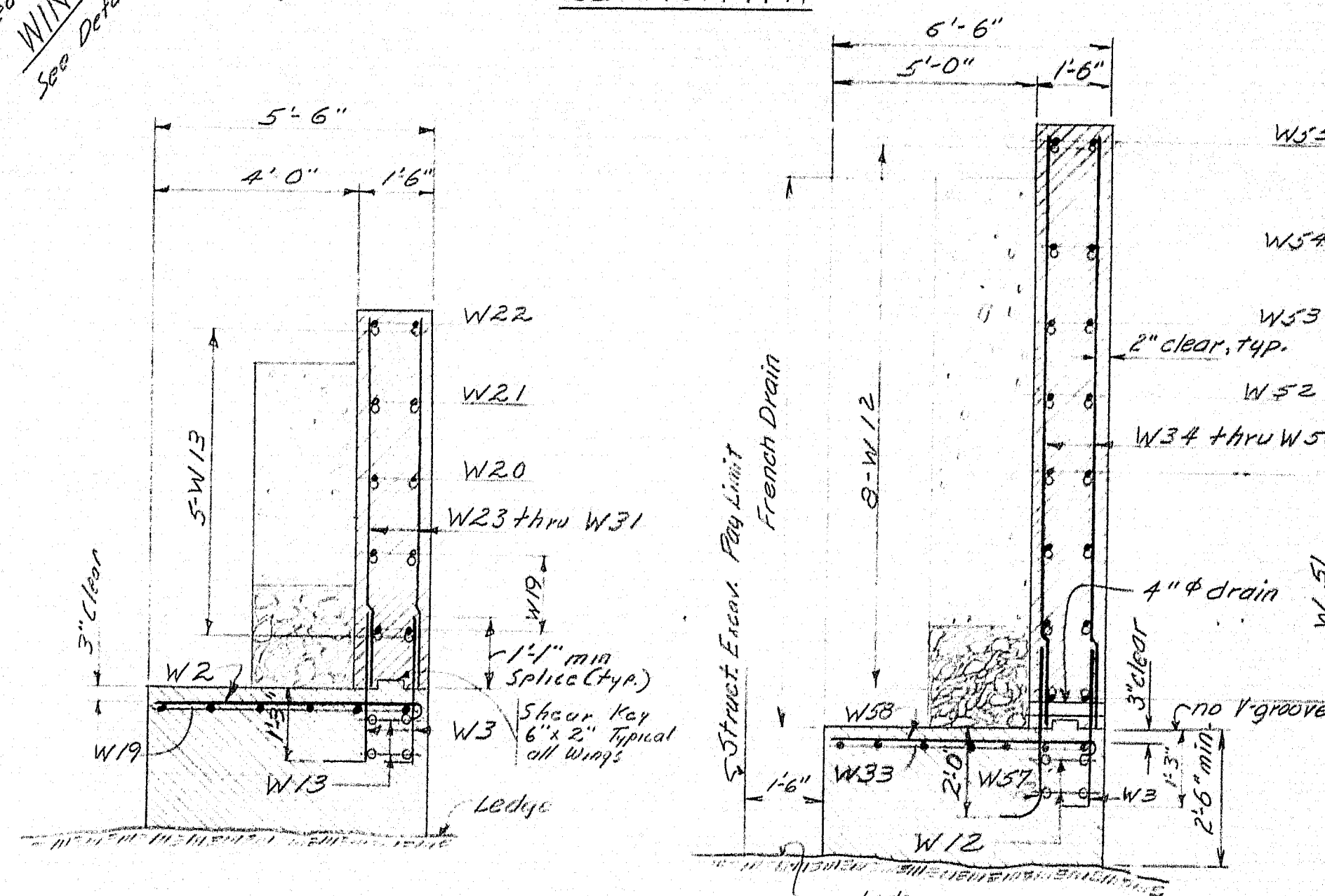
At all Vertical Construction Joints, terminate shear key and water stop 4" below finished grade. At wing joints the key is to project into the wing section and shall not be reversed.



FRONT ELEVATION



SECTION A-A



SECTION B-B

SECTION C-C

Details similar to Section C-C  
(See revisions to West Wing)

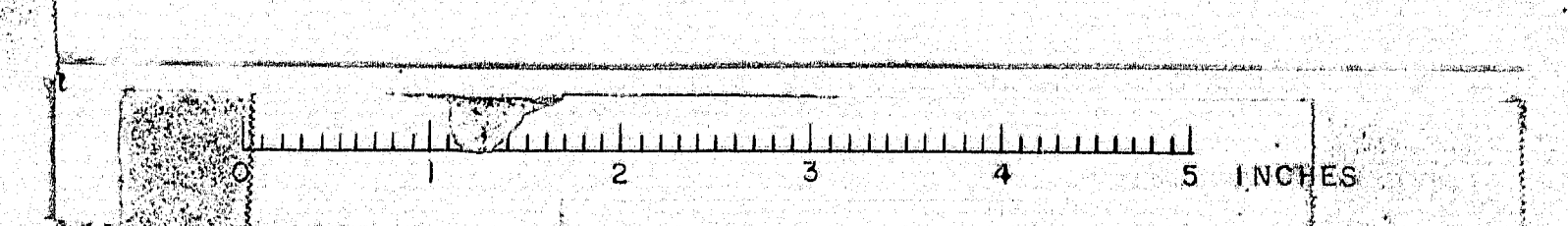
(See revisions to East Wing)  
Ledge revised 5/18/67 W.H. 1/253

See Sheet #3 for General Abutment Notes  
REVISION NOTES

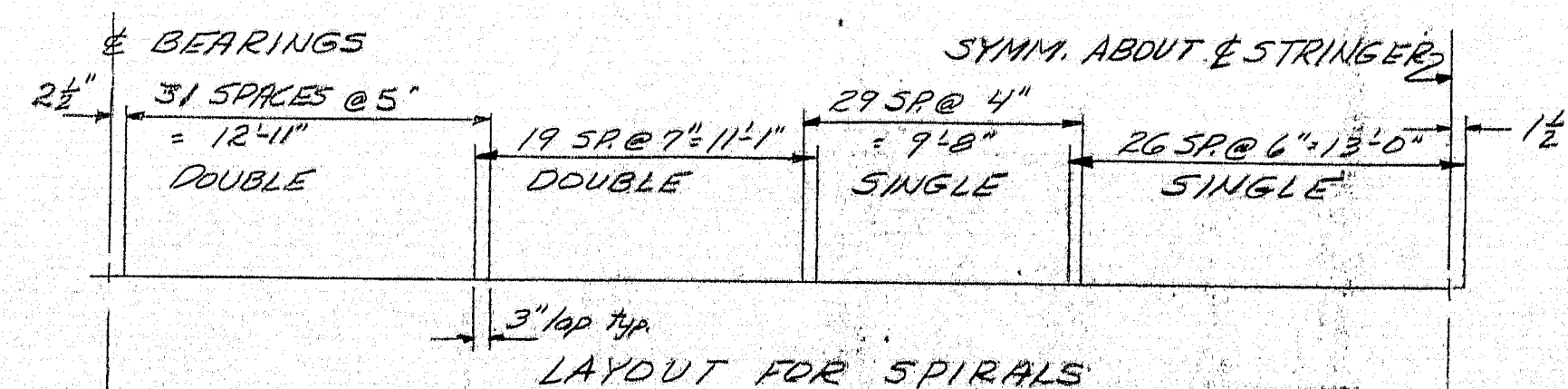
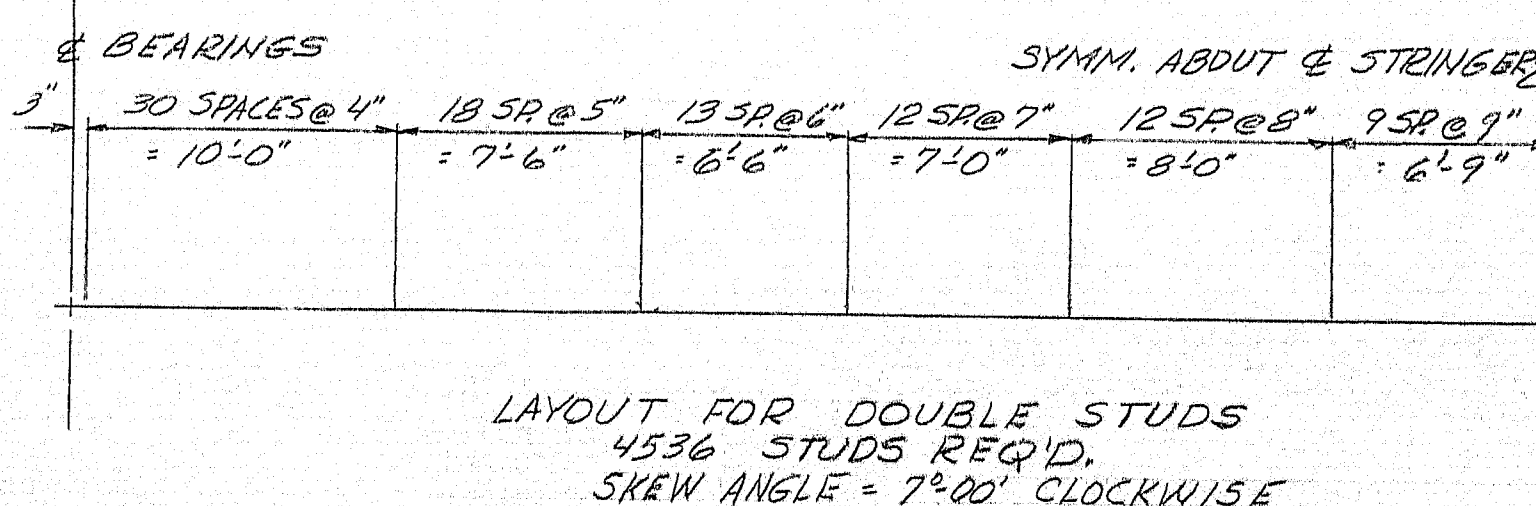
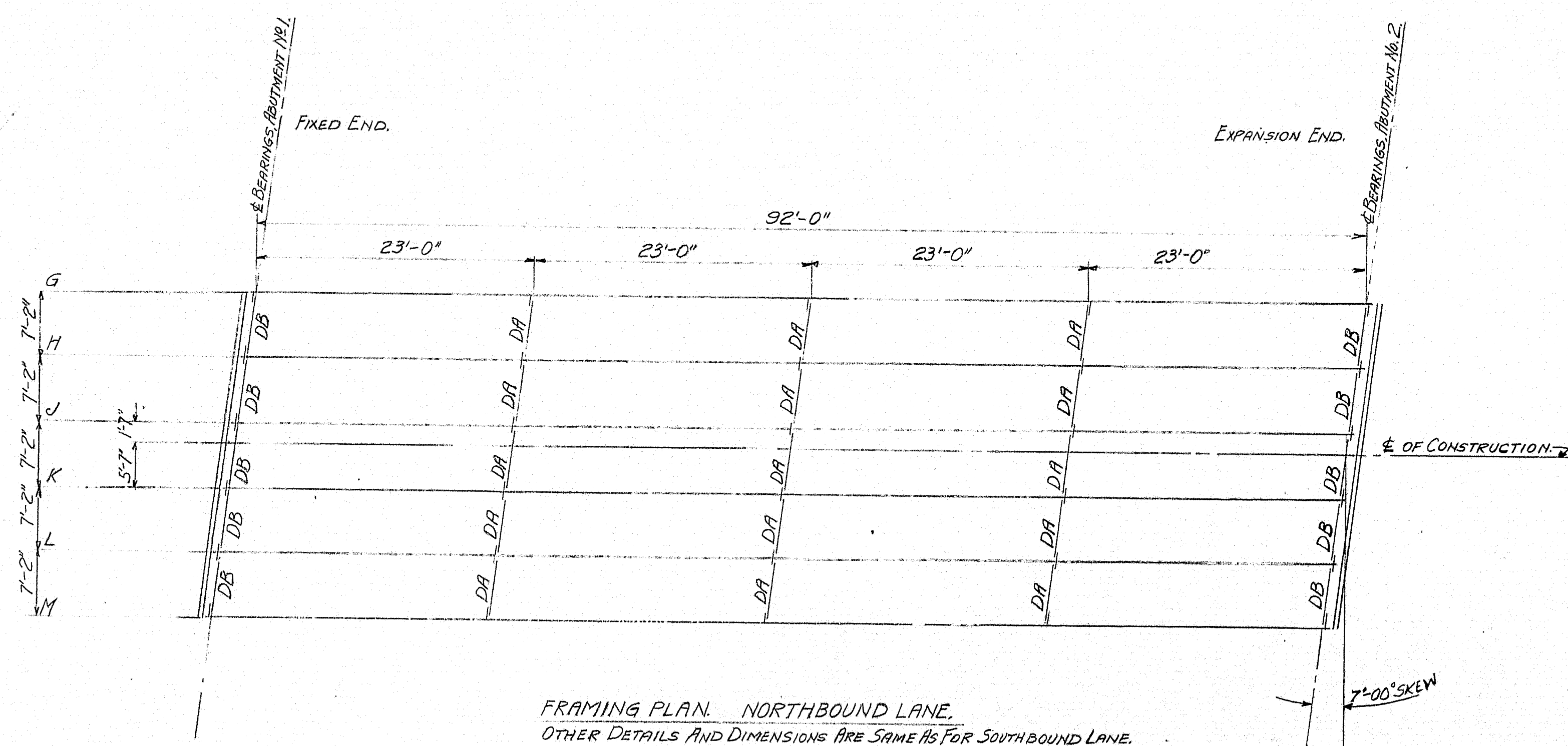
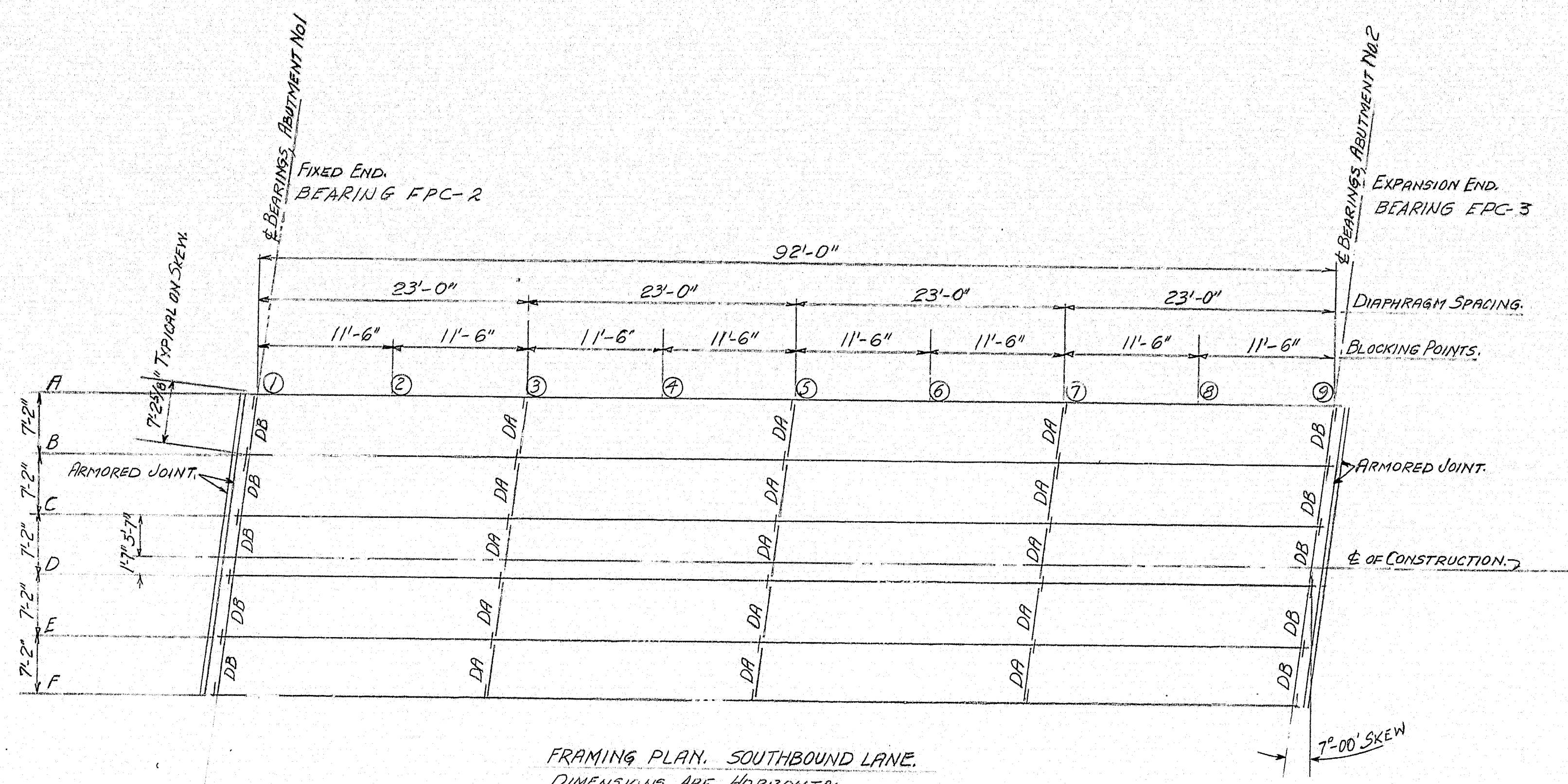
- Splice bars are not shown in Sections A-A, B-B & C-C. Additional reinforcing steel required to be cut from random length bars shown below:
- |                   |  |
|-------------------|--|
| 1-#5 bar x 24'-0" | (1.6 x 4'-0")  |
| 3-#5 " x 27'-0"   | (3.6 x 7'-0") + (1.6 x 5'-0"); 4(6'-8")                    |
| 2-#5 " x 29'-0"   | (2.5 x 0") + (6 x 3'-11"); 6(3'-11") + 2(2'-9") + 2(2'-2") |
| 2-#5 " x 31'-0"   | 4(7'-6") + 2(8'-3") + 2(7'-0")                             |
| 1-#5 " x 33'-0"   | 1(14'-0") + 1(18'-6")                                      |
| 1-#5 " x 35'-0"   | 2(7'-6") + 2(10'-11")                                      |
| 1-#5 " x 37'-0"   | 4(9'-2")   |
| 2-#5 " x 39'-0"   | 1(38'-11") + 1(38'-11")                                    |
| 2-#5 " x 40'-0"   | 9(4'-6") + 9(4'-6")  |
| 1-#6 bar x 32'-0" | 9(3'-0") + 1(5'-0")  |
| 1-#5 " x 27'-0"   | 9(3'-0")   |
- A20 = 8'-3"  
A21 = 9'-2"  
W12 = 7'-0"  
W13 = 6'-7"

DESIGN: A.B.S.	BRIDGE NO. 1253
CHECK: H.L.D.	PLOT:
STATE HIGHWAY COMMISSION BRIDGE DIVISION <b>INTERSTATE 95</b> OVER <b>ROUTE 158</b> IN THE TOWN OF <b>SHERMAN</b> ARROSTOCK COUNTY ABUTMENT 2, N.B.	
SHEET 6 OF 10 AUGUSTA, MAINE APRIL 1966	

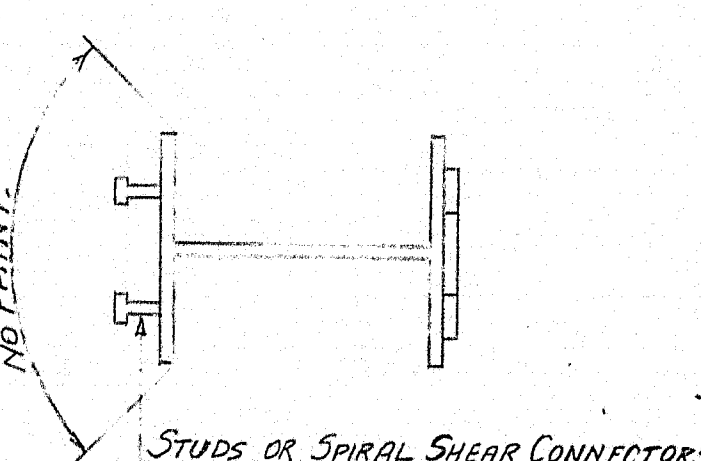
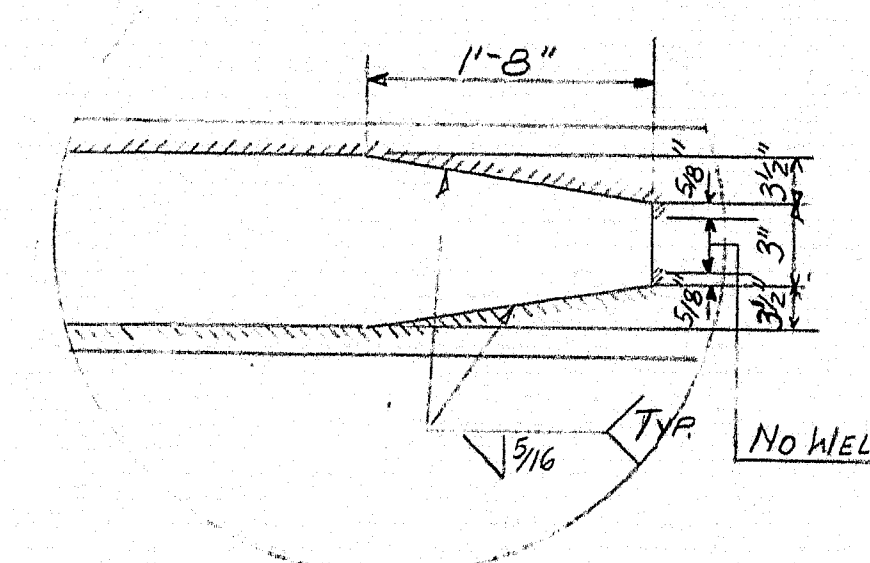
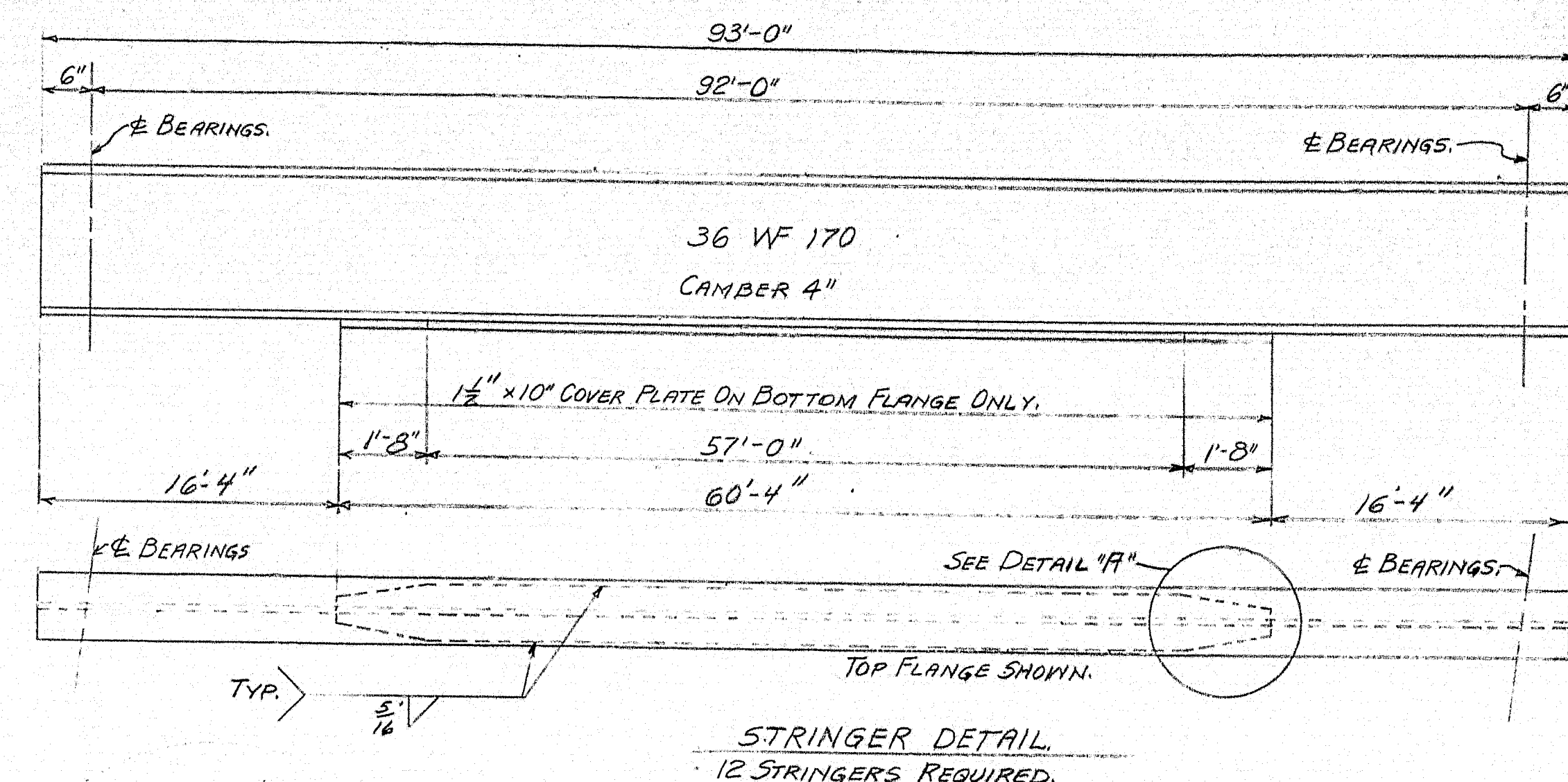
M-2523



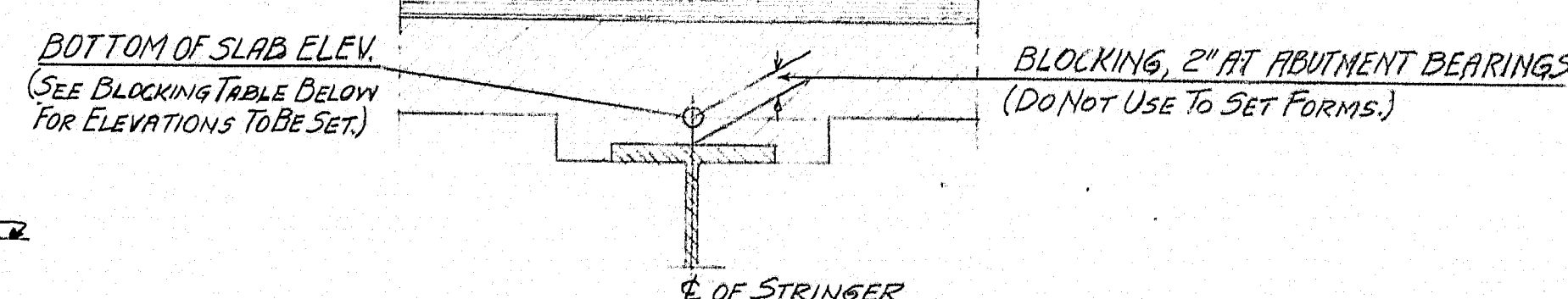




SHEAR CONNECTORS  
TYPICAL FOR EACH STRINGER



- NOTES
1. Set Diaphragms normal to grade.
  2. Armored joints shall consist of two elements with a joint at construction.
  3. All dimensions are horizontal.



STRINGER	1	2	3	4	5	6	7	8	9
A	516.81	516.74	517.06	517.12	517.15	517.18	517.13	517.67	516.86
B	516.91	517.06	517.16	517.25	517.30	517.29	517.26	517.19	517.09
C	517.02	517.16	517.20	517.36	517.41	517.40	517.37	517.30	517.21
D	517.06	517.21	517.32	517.41	517.46	517.47	517.44	517.37	517.28
E	516.92	517.06	517.14	517.27	517.32	517.33	517.30	517.24	517.15
F	516.77	516.92	517.05	517.14	517.19	517.20	517.16	517.11	517.02

STRINGER	1	2	3	4	5	6	7	8	9
G	512.50	512.64	512.77	512.86	512.91	512.92	512.90	512.84	512.76
H	512.62	512.76	512.89	512.98	513.03	513.05	513.02	512.96	512.88
J	512.74	512.88	513.01	513.10	513.16	513.17	513.14	513.08	513.00
K	512.67	512.81	512.94	513.03	513.08	513.10	513.07	513.01	512.93
L	512.54	512.68	512.81	512.90	512.96	512.97	512.94	512.88	512.81
M	512.41	512.56	512.68	512.78	512.83	512.84	512.82	512.76	512.68

STRUCTURAL STEEL CLASSIFICATION  
ASTM-A441 FOR STRINGERS AND COVER  
PLATES. ASTM-A36 FOR BEARINGS,  
SHEAR CONNECTORS, ARMORED JOINTS, AND  
DIAPHRAGMS, EXCEPT AS NOTED ON  
THE STANDARD DETAILS.

REFERENCES  
BEARINGS BD 101-64  
REQ'D 12 EPC-2, 12 EPC-3  
DIAPHRAGMS BD 104-66  
REQ'D 30 TYPE A 20 TYPE B  
ARMORED JOINT BD 104-66  
REQ'D 4  
SHEAR CONNECTORS - BD 104-66  
DRAINS - Sheet 9 and BD-104-66

DESIGN - PMW  
TRACE - JLF  
CHECK - J. CHANDLER

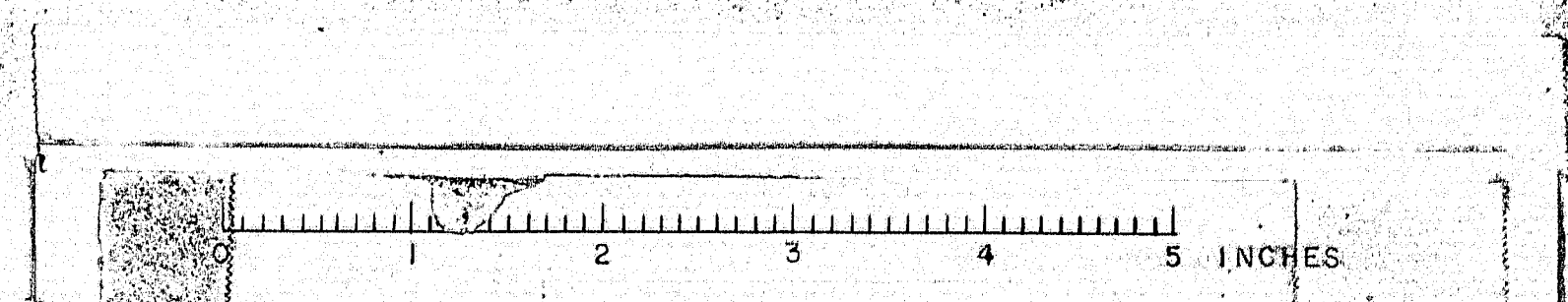
BRIDGE NO.  
SURVEY  
PLOT

STATE HIGHWAY COMMISSION  
BRIDGE DIVISION  
**INTERSTATE 95**  
OVER  
**ROUTE 158**  
IN THE TOWN OF  
**SHERMAN**  
**AROOSTOOK COUNTY**  
STRUCTURAL STEEL

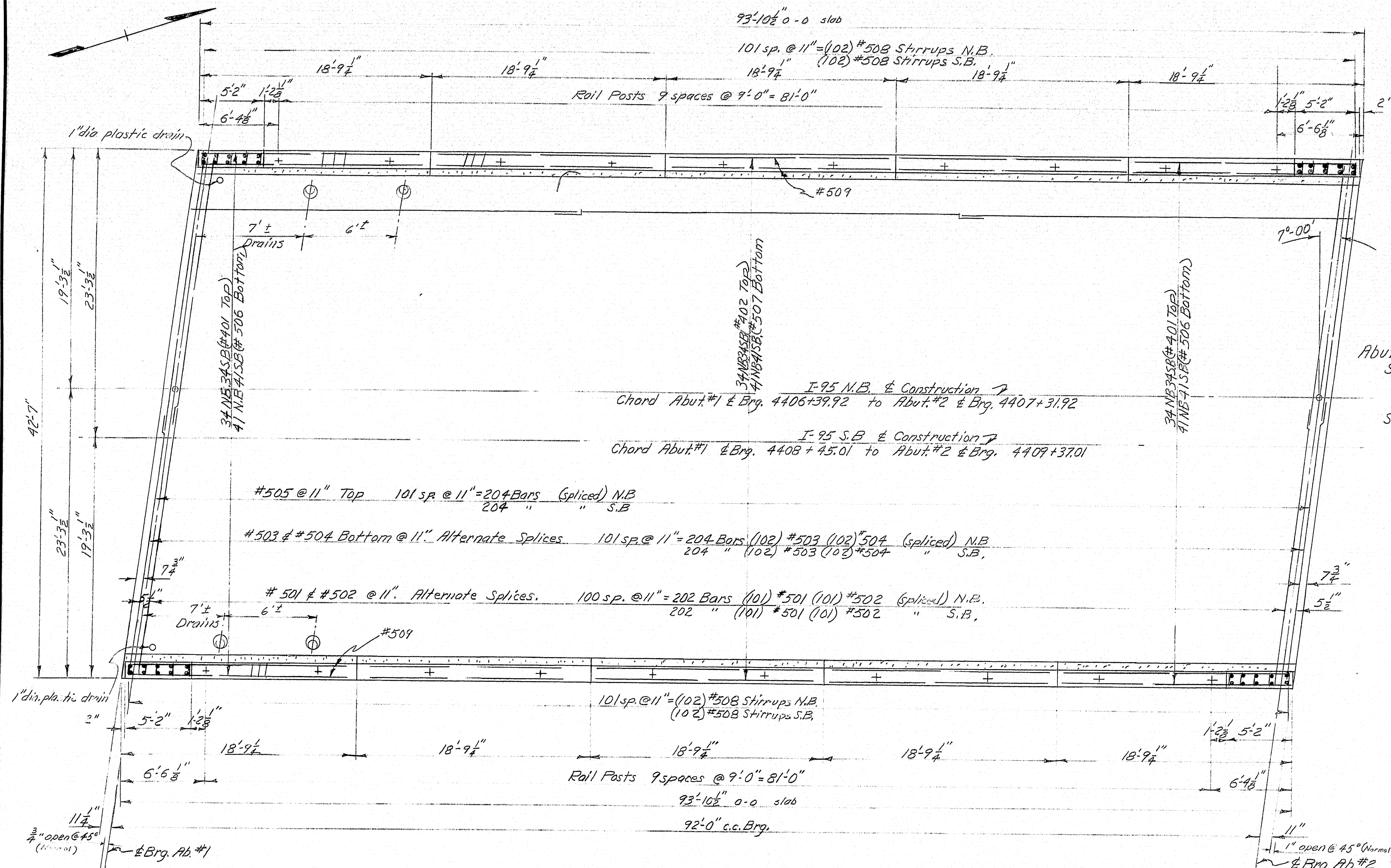
Revised - Table of Bottom of Slab  
Elevations - Southbound Lane 8-16-67

SHEET 7 OF 10 AUGUSTA, MAINE APRIL 1966

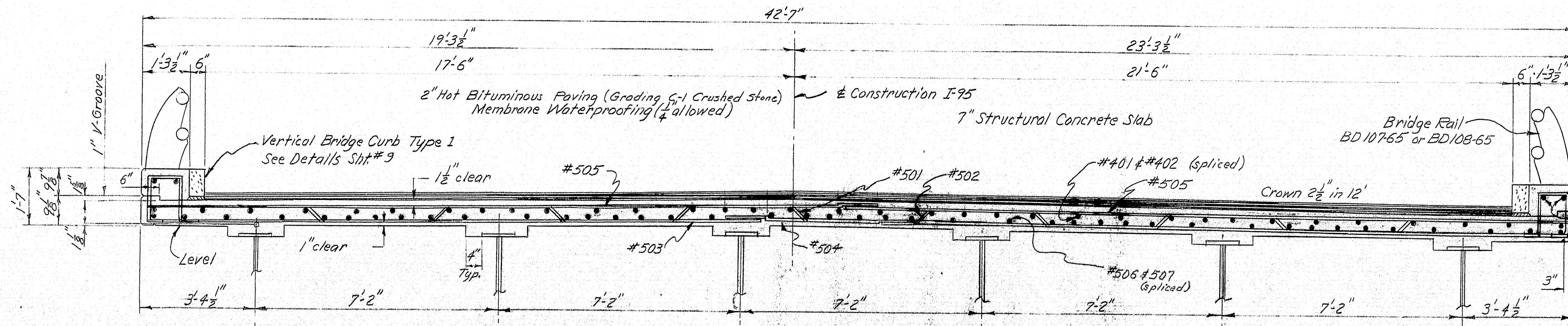
M-2524



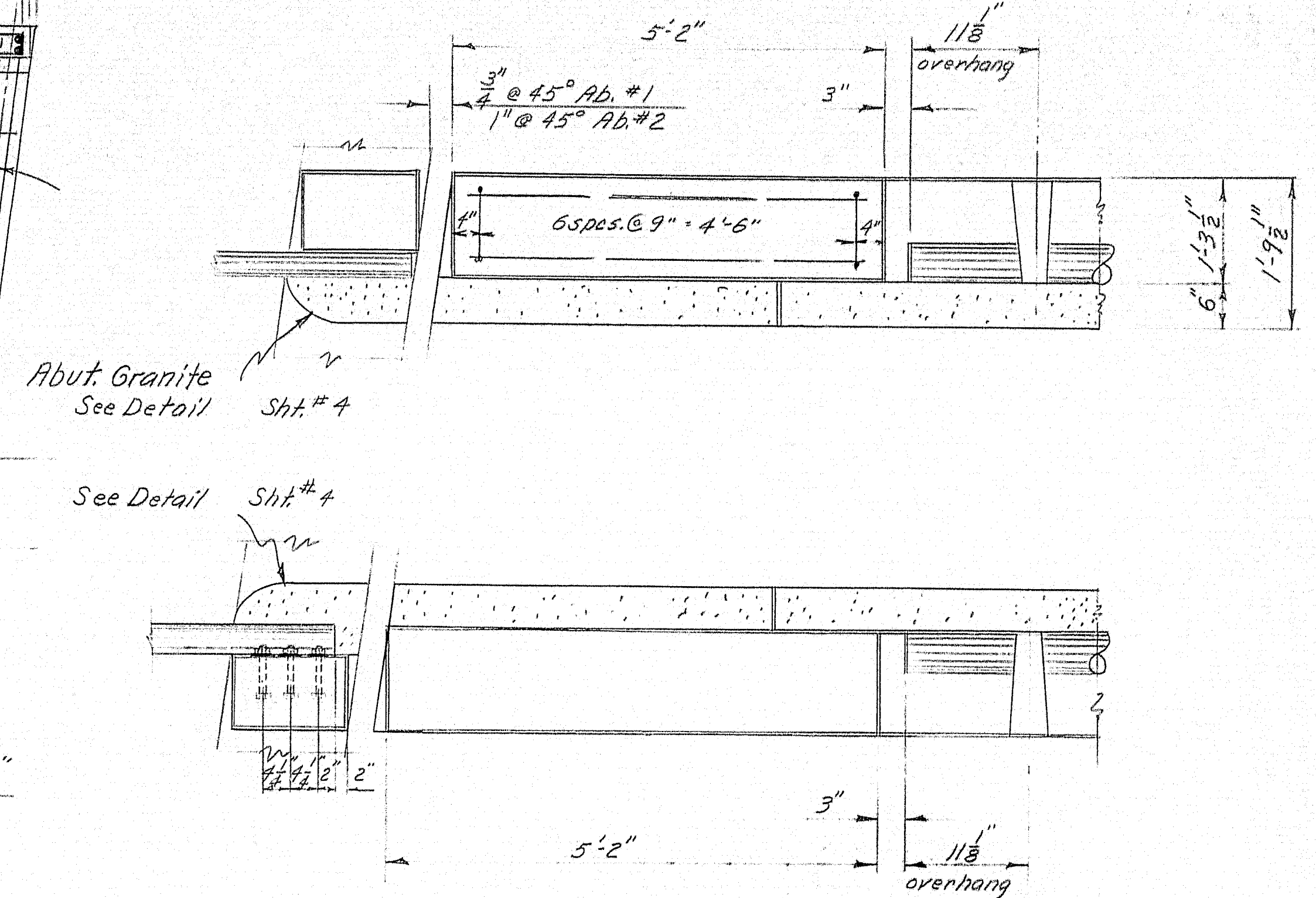




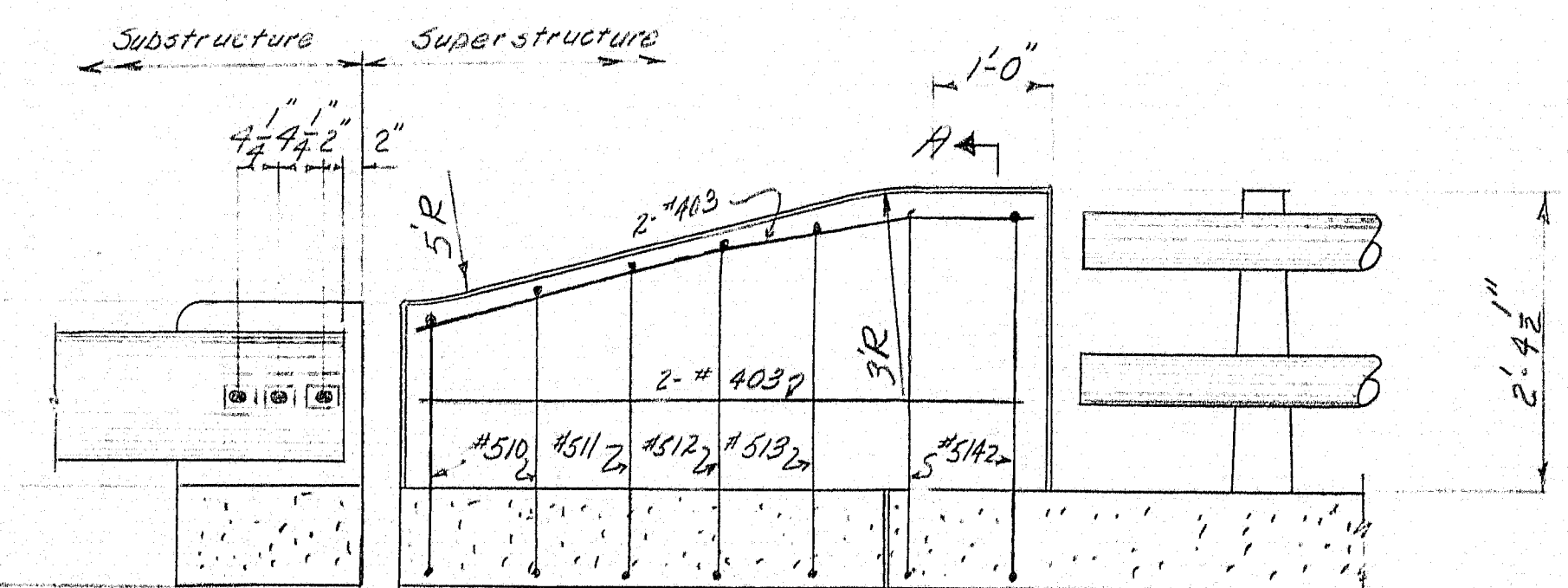
**SUPERSTRUCTURE**  
**NORTHBOUND and SOUTHBOUND**



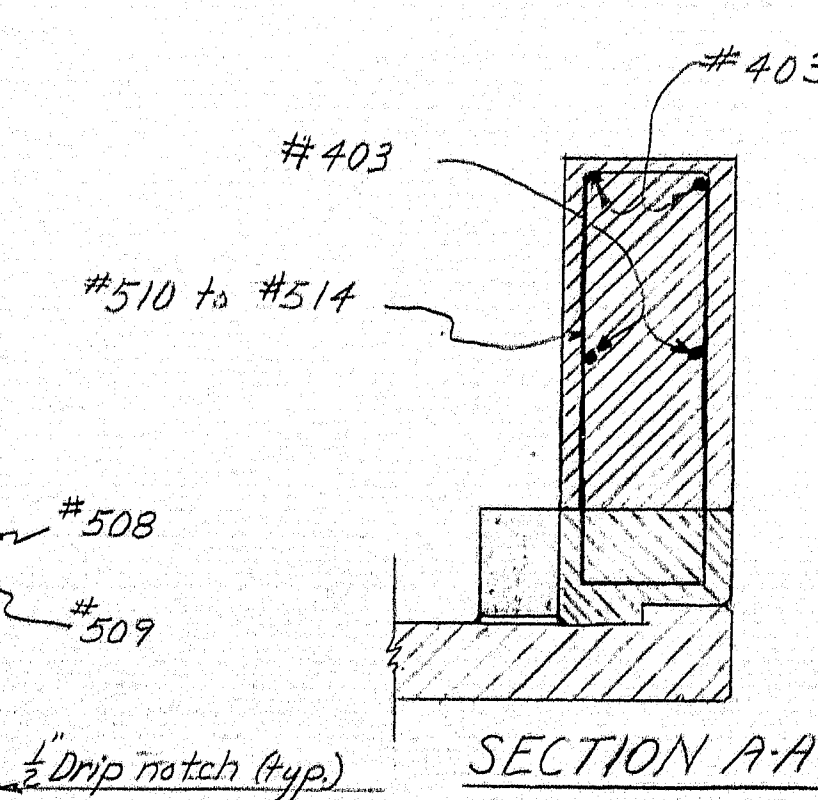
**TRANSVERSE SECTION WITH TRAFFIC**



**PLAN**



**END POST DETAIL**

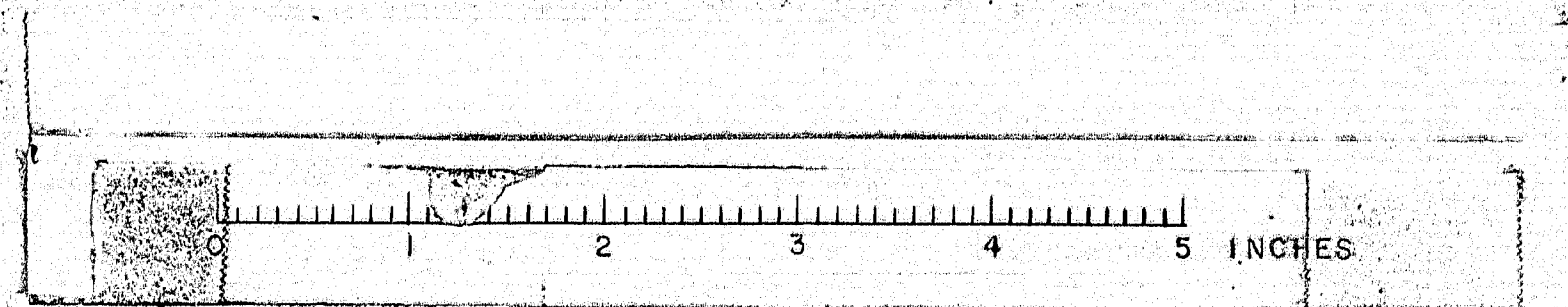


**SECTION A-A**

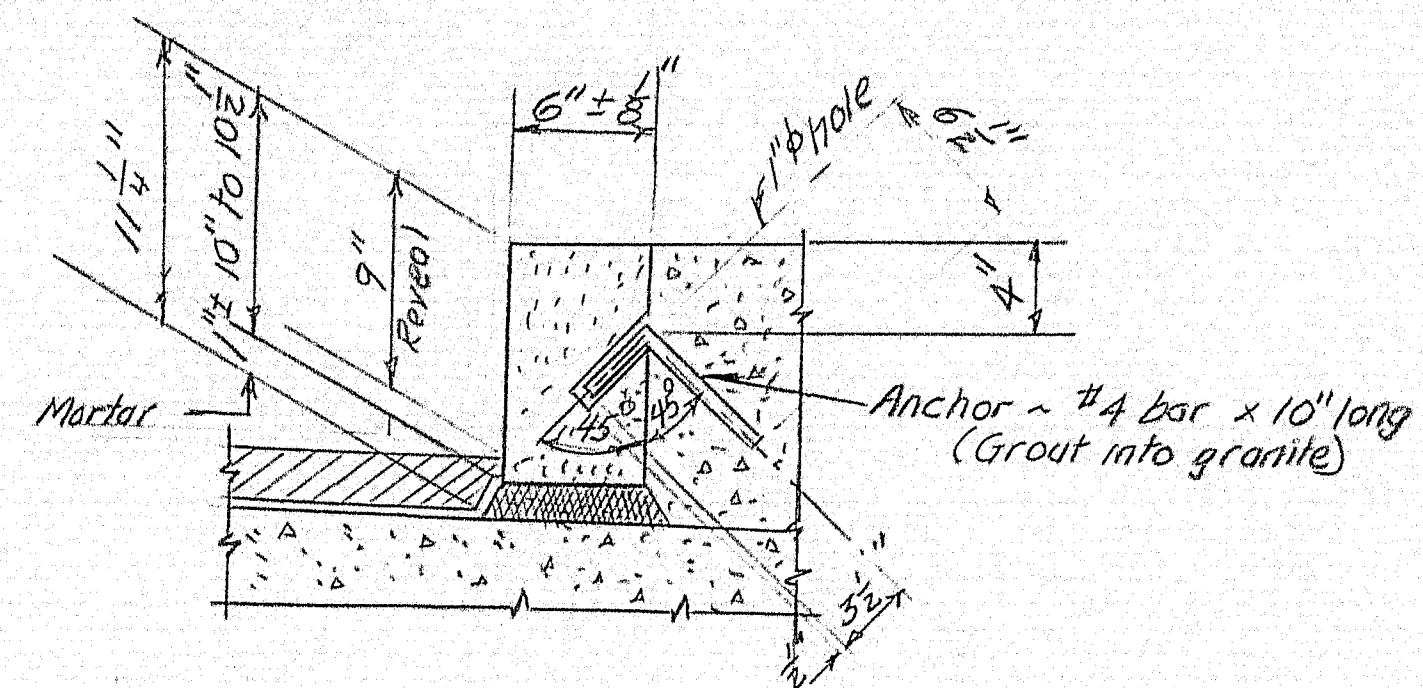
Concrete in superstructure and post to be paid for under Item 502.26 Structural Concrete. See Sh #4 for details of substructure and post.

DESIGN - B.M.W. TRACE - S. DET. - E.V.S. CHECK - S. & S. & S.	BRIDGE NO. SURVEY - PLOT -
STATE HIGHWAY COMMISSION BRIDGE DIVISION	
<b>INTERSTATE 95</b>	
OVER	
<b>ROUTE 158</b>	
IN THE TOWN OF	
<b>SHERMAN</b>	
<b>AROOSTOOK COUNTY</b>	
SUPERSTRUCTURE	
SHEET 8 OF 10	AUGUSTA, MAINE
APRIL 1966	

M-2525







GRANITE CURB SECTION

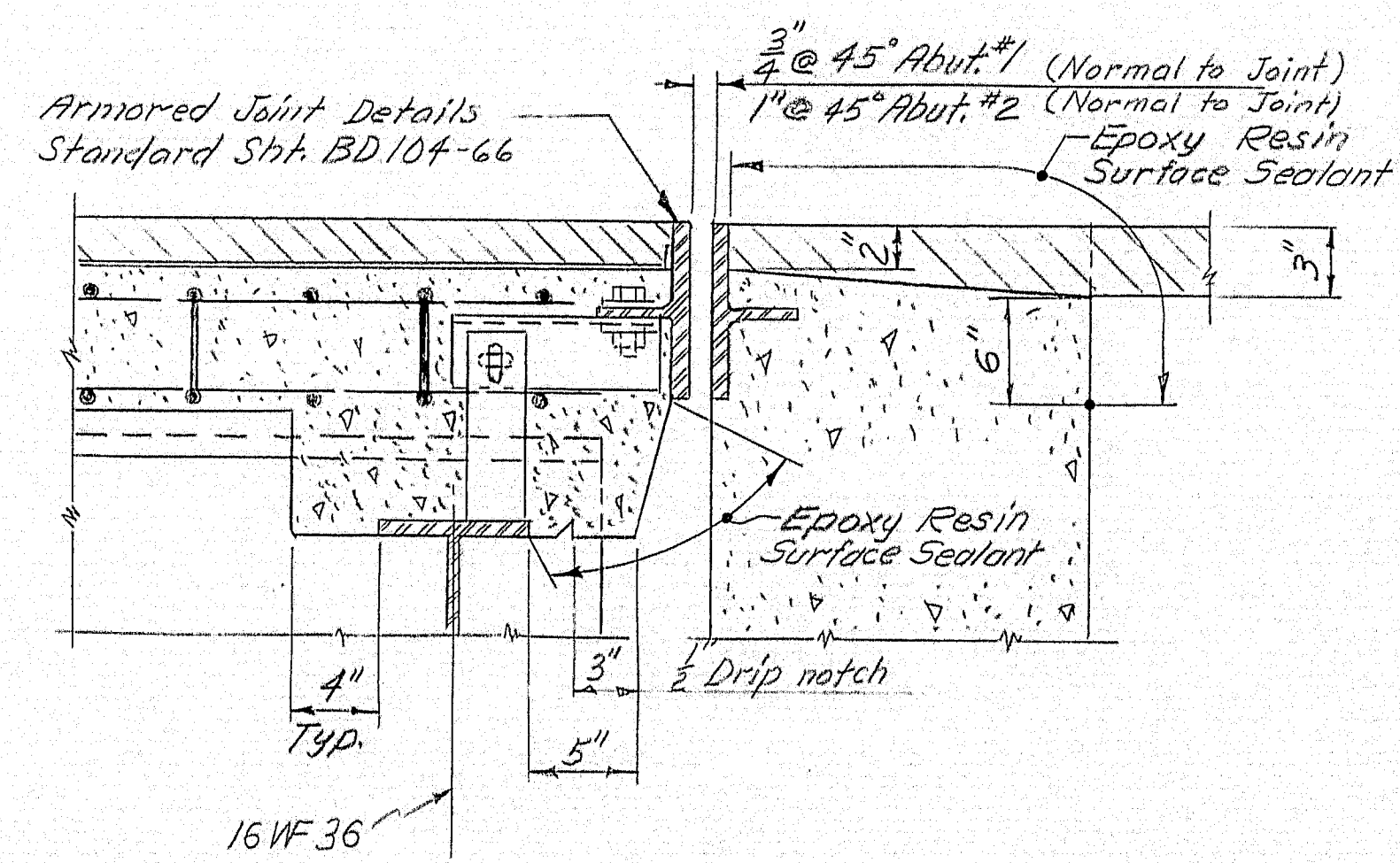
### SUPERSTRUCTURE NOTES

All reinforcing to be 2" clear unless otherwise noted.

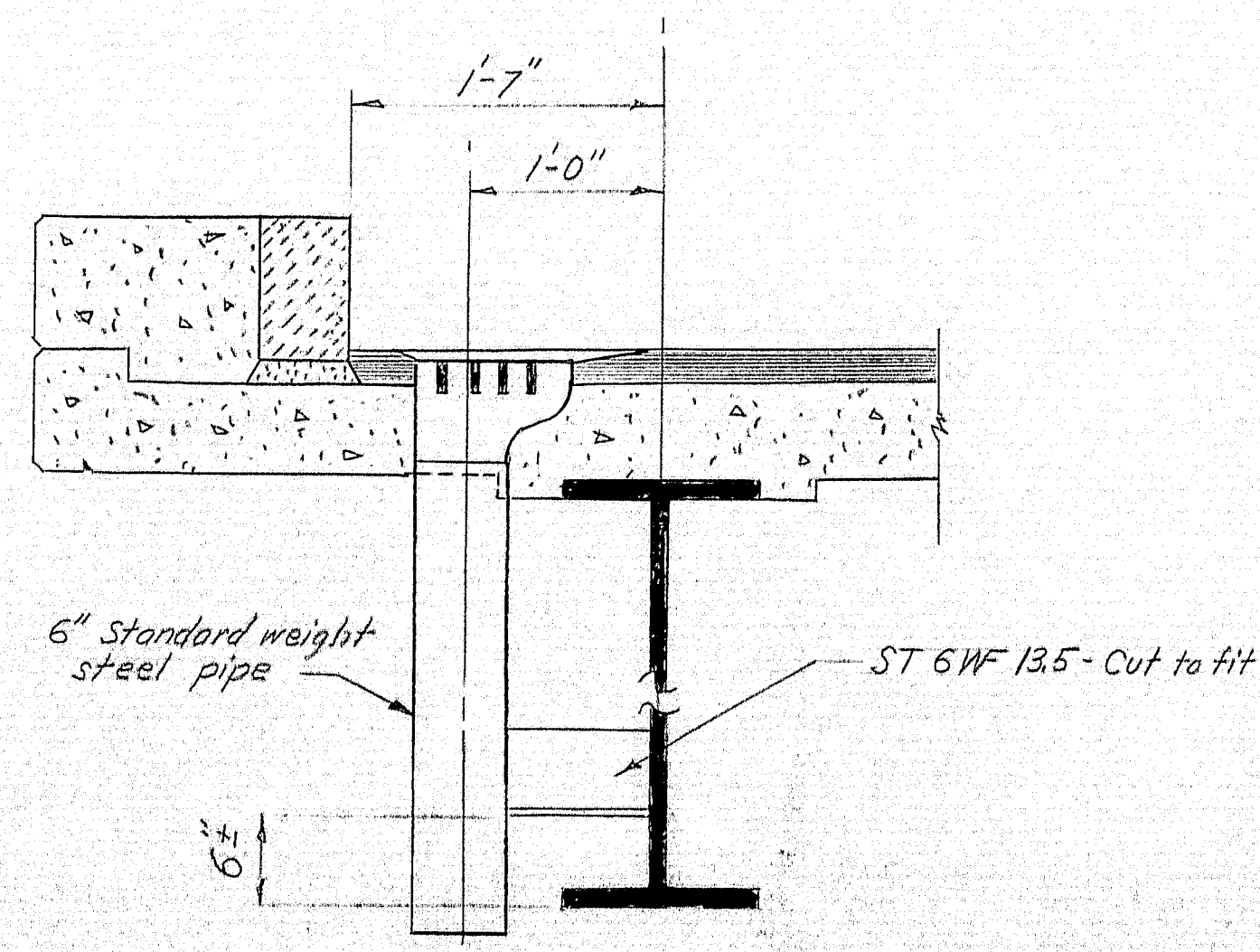
Form a 1" V-groove on outside face of curb and slab at each contraction joint.

Reinforcing steel should not extend thru contraction joint in concrete curb.

Break the bond between concrete surfaces at contraction joints by coating contact areas with a suitable grade of asphalt paint.



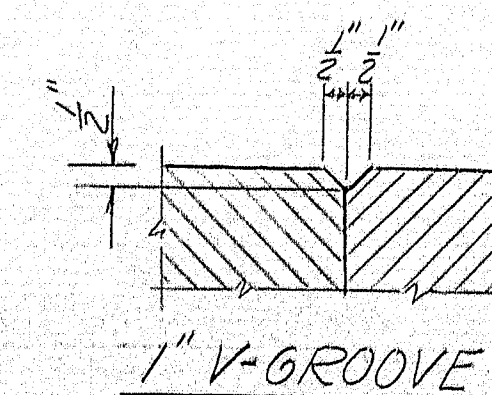
SECTION AT ABUTS.



DRAIN DETAIL

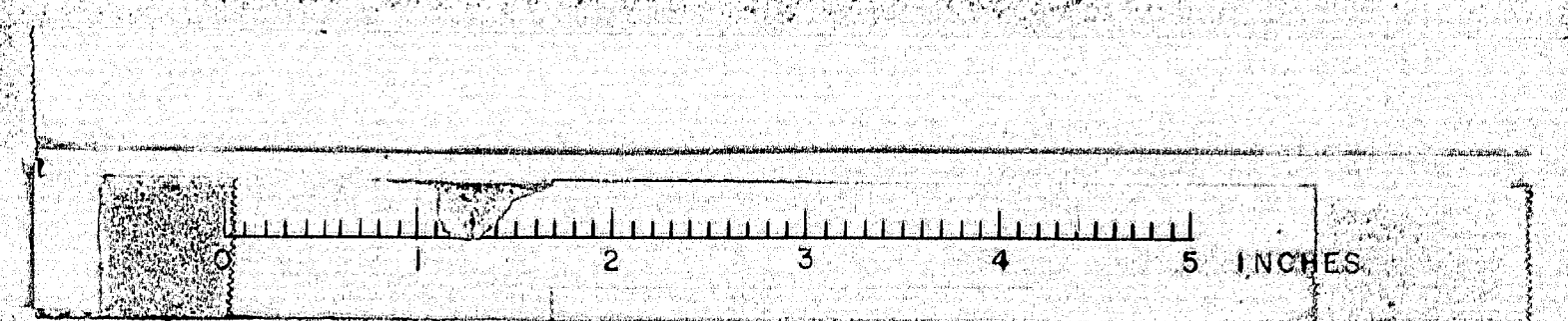
8 REQUIRED

NOTE: For Fabrication & welding detail, see Standard Sheet BD 104-66.

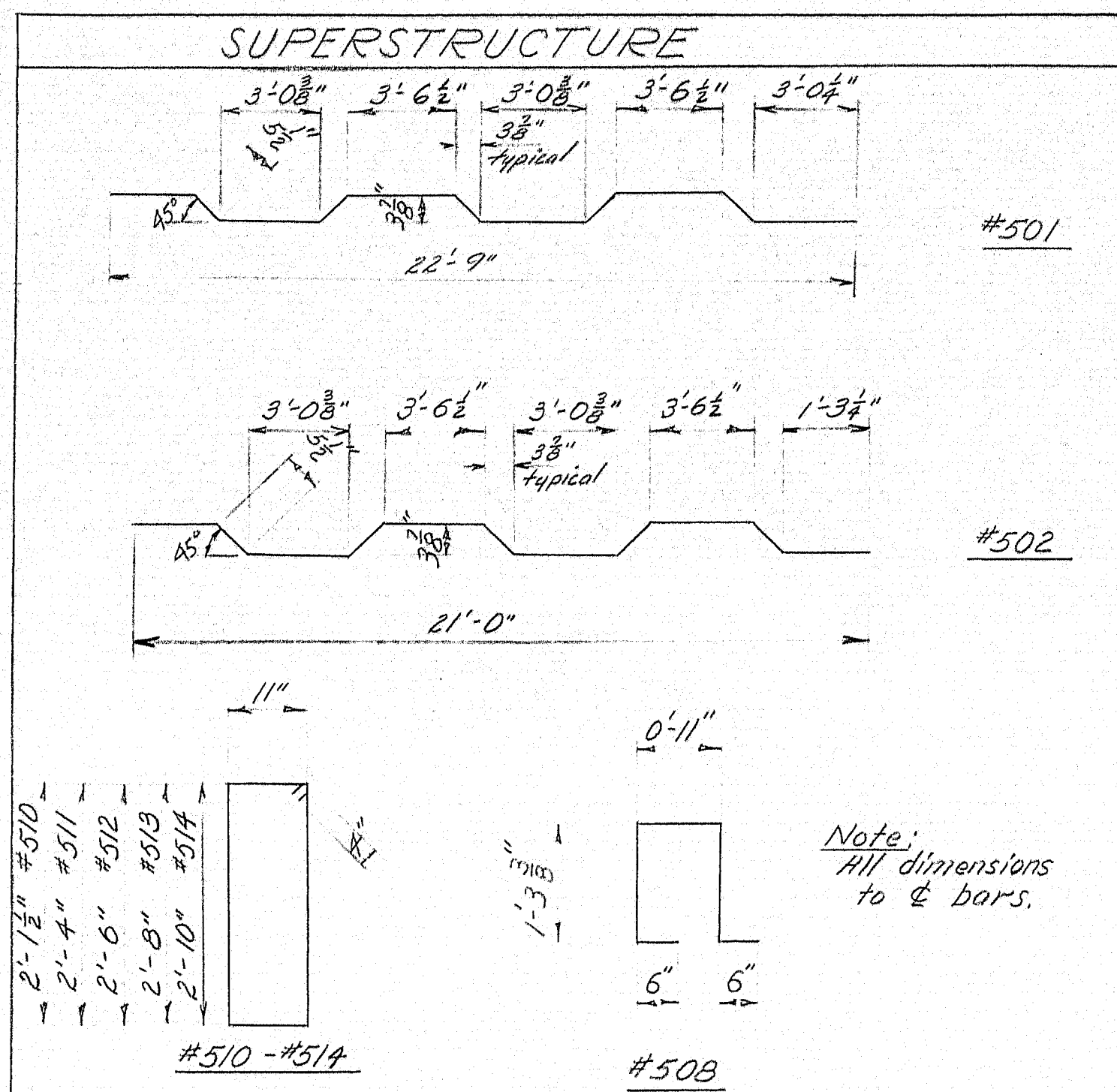


DESIGN - PMW	BRIDGE NO.
TRACE & DET. - E.V.S.	SURVEY -
CHECK - U. CHANDLER	PLOT -
STATE HIGHWAY COMMISSION	
BRIDGE DIVISION	
INTERSTATE 95	
OVER	
ROUTE 158	
IN THE TOWN OF	
SHERMAN	
AROOSTOOK COUNTY	
SUPERSTRUCTURE DETAILS	
SHEET 9 OF 10	AUGUSTA, MAINE
	APRIL 1966

M-2526





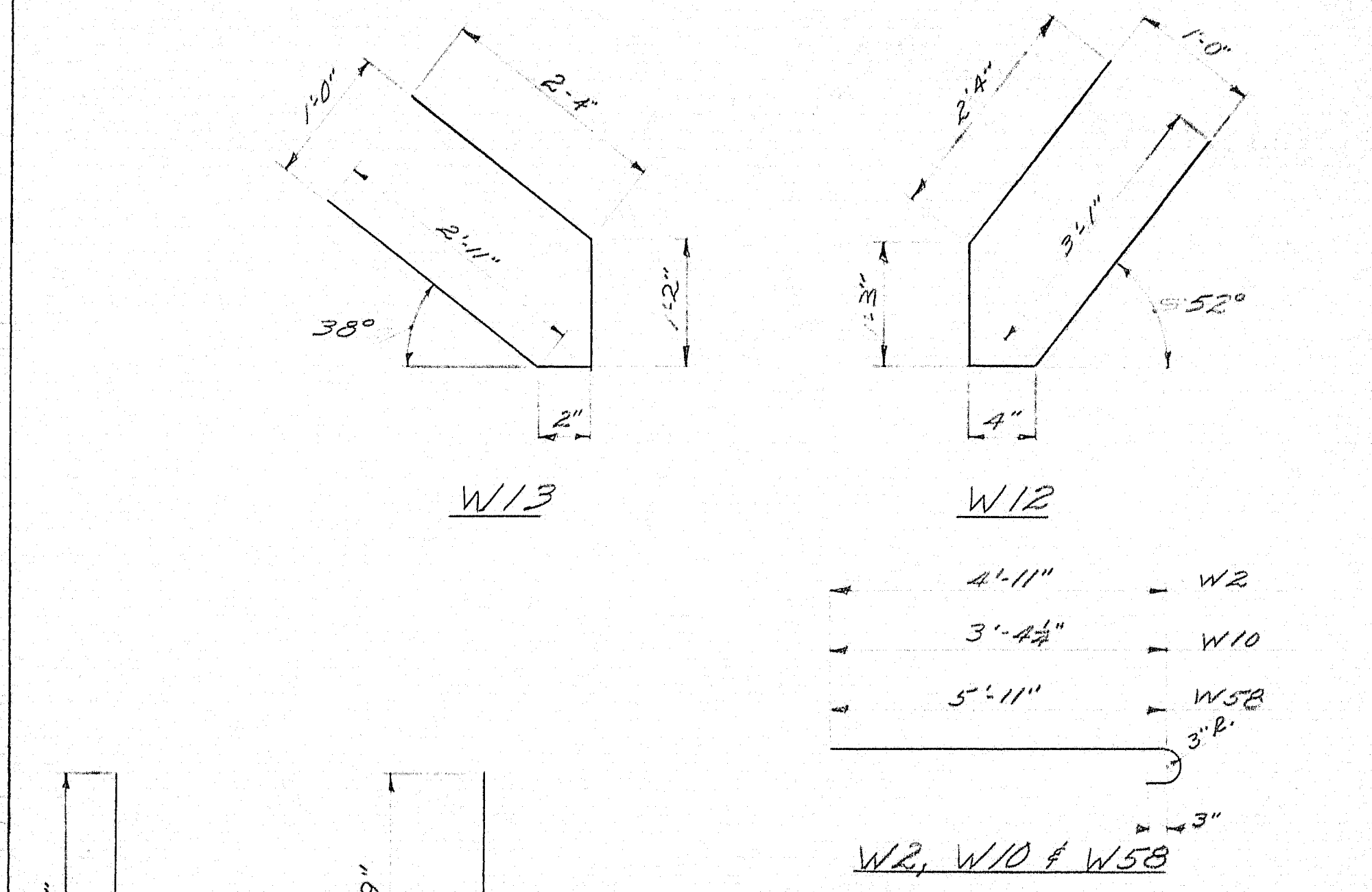
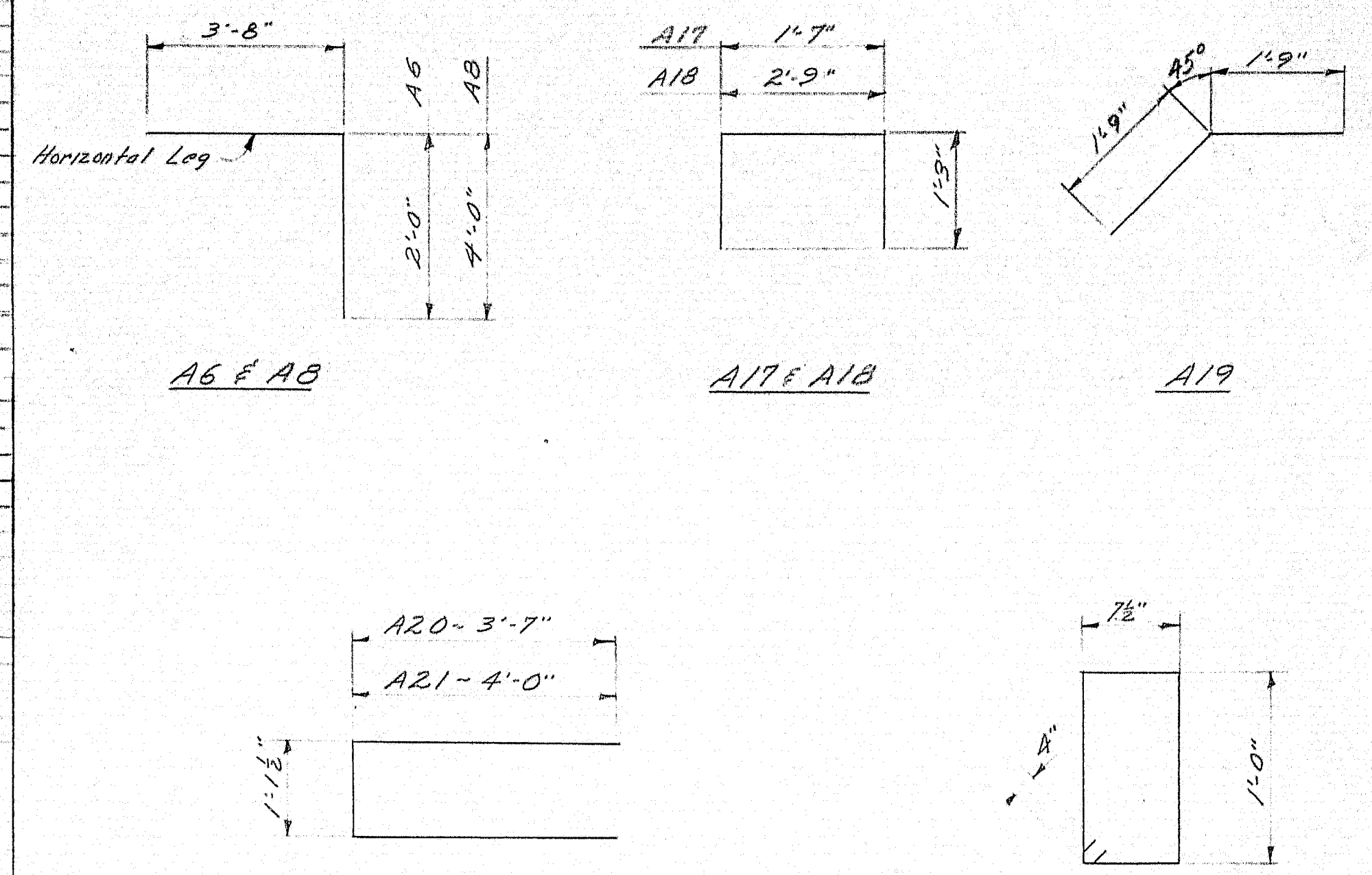


BENT BARS				
DESIG.	SIZE	NO.	LENGTH	LOCATION
#501	#5	202	23'-6"	Slab Transverse
#502	#5	202	21'-9"	"
#508	#5	408	4'-6"	Curb Stirrups
#510	#5	16	6'-9"	End Post Stirrups
#511	#5	8	7'-2"	" " "
#512	#5	8	7'-6"	" " "
#513	#5	8	7'-10"	" " "
#514	#5	16	8'-2"	" " "

STRAIGHT BARS				
DESIG.	SIZE	NO.	LENGTH	LOCATION
#401	#4	136	30'-0"	Slab Longitudinal Top
#402	#4	68	35'-7"	" " "
#403	#4	32	4'-10"	End posts horizontal
#503	#5	204	18'-3"	Slab transverse Bottom
#504	#5	204	25'-5"	" " "
#505	#5	408	21'-10"	" " Top
#506	#5	164	30'-0"	" Longitudinal Bottom
#507	#5	82	35'-7"	" " "
#509	#5	40	18'-5"	Curb Longitudinal

E.V.S.  
J.C.

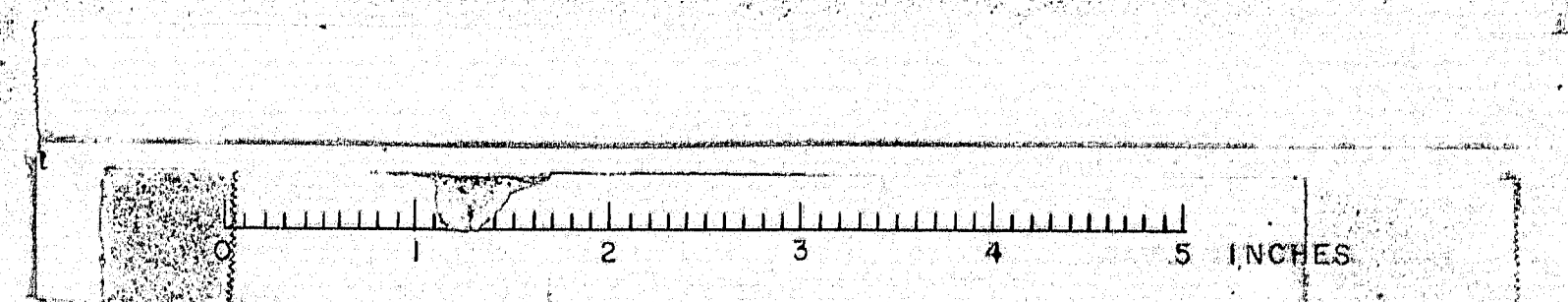
ABUTMENTS									
BENT BARS					STRAIGHT BARS				
MARK	SIZE	LENGTH	NO.	LOCATION	MARK	SIZE	LENGTH	NO.	LOCATION
A6	#5	3'-0"	5	Abutment	A1	#5	38'-11"	6	Breastwall
A7	#5	3'-0"	5	Abutment	A2	#5	18'-0"	1	Breastwall
A17	#5	3'-0"	5	Abutment	A3	#5	8'-0"	1	Breastwall
A18	#5	3'-0"	5	Abutment	A4	#5	26'-0"	1	Breastwall
A19	#5	3'-0"	5	Abutment	A5	#5	21'-6"	1	Breastwall
A20	#5	3'-0"	5	Abutment	A7	#5	2'-6"	57	Breastwall & Backwall
A21	#5	3'-0"	5	Abutment	A9	#5	5'-6"	5	Breastwall
A22	#5	3'-0"	5	Abutment	A10	#5	3'-0"	16	Breastwall
W2	#5	3'-0"	5	Wing Wall	A11	#5	4'-9"	6	Breastwall
W3	#5	3'-0"	5	Wing Wall	A12	#5	10'-6"	6	Breastwall
W10	#5	3'-0"	5	Wing Wall	A13	#5	6'-8"	6	Breastwall
W12	#5	3'-0"	5	Wing Wall	A14	#5	4'-10"	64	Backwall
W13	#5	3'-0"	5	Wing Wall	A15	#5	38'-11"	8	Backwall
W17	#5	3'-0"	5	Wing Wall	A16	#5	3'-0"	8	Substructure Post
W18	#5	3'-0"	5	Wing Wall	A22	#5	3'-0"	8	Substructure Post
W19	#5	3'-0"	5	Wing Wall	W1	#5	8'-2"	20	Wing Wall
W20	#5	3'-0"	5	Wing Wall	W4	#5	2'-9"	4	Wing Wall
W21	#5	3'-0"	5	Wing Wall	W5	#5	3'-3"	4	Wing Wall
W22	#5	3'-0"	5	Wing Wall	W6	#5	3'-10"	4	Wing Wall
W23	#5	3'-0"	5	Wing Wall	W7	#5	4'-4"	8	Wing Wall
W24	#5	3'-0"	5	Wing Wall	W8	#5	4'-10"	8	Wing Wall
W25	#5	3'-0"	5	Wing Wall	W9	#5	5'-5"	4	Wing Wall
W26	#5	3'-0"	5	Wing Wall	W11	#5	8'-7"	4	Wing Wall
W27	#5	3'-0"	5	Wing Wall	W14	#5	3'-0"	8	Wing Wall
W28	#5	3'-0"	5	Wing Wall	W15	#5	3'-0"	8	Wing Wall
W29	#5	3'-0"	5	Wing Wall	W16	#5	3'-0"	8	Wing Wall
W30	#5	3'-0"	5	Wing Wall	W17	#5	3'-0"	8	Wing Wall
W31	#5	3'-0"	5	Wing Wall	W18	#5	3'-0"	8	Wing Wall
W32	#5	3'-0"	5	Wing Wall	W19	#5	3'-0"	8	Wing Wall
W33	#5	3'-0"	5	Wing Wall	W20	#5	3'-0"	8	Wing Wall
W34	#5	3'-0"	5	Wing Wall	W21	#5	3'-0"	8	Wing Wall
W35	#5	3'-0"	5	Wing Wall	W22	#5	3'-0"	8	Wing Wall
W36	#5	3'-0"	5	Wing Wall	W23	#5	3'-0"	8	Wing Wall
W37	#5	3'-0"	5	Wing Wall	W24	#5	3'-0"	8	Wing Wall
W38	#5	3'-0"	5	Wing Wall	W25	#5	3'-0"	8	Wing Wall
W39	#5	3'-0"	5	Wing Wall	W26	#5	3'-0"	8	Wing Wall
W40	#5	3'-0"	5	Wing Wall	W27	#5	3'-0"	8	Wing Wall
W41	#5	3'-0"	5	Wing Wall	W28	#5	3'-0"	8	Wing Wall
W42	#5	3'-0"	5	Wing Wall	W29	#5	3'-0"	8	Wing Wall
W43	#5	3'-0"	5	Wing Wall	W30	#5	3'-0"	8	Wing Wall
W44	#5	3'-0"	5	Wing Wall	W31	#5	3'-0"	8	Wing Wall
W45	#5	3'-0"	5	Wing Wall	W32	#5	3'-0"	8	Wing Wall
W46	#5	3'-0"	5	Wing Wall	W33	#5	3'-0"	8	Wing Wall
W47	#5	3'-0"	5	Wing Wall	W34	#5	3'-0"	8	Wing Wall
W48	#5	3'-0"	5	Wing Wall	W35	#5	3'-0"	8	Wing Wall
W49	#5	3'-0"	5	Wing Wall	W36	#5	3'-0"	8	Wing Wall
W50	#5	3'-0"	5	Wing Wall	W37	#5	3'-0"	8	Wing Wall
W51	#5	3'-0"	5	Wing Wall	W38	#5	3'-0"	8	Wing Wall
W52	#5	3'-0"	5	Wing Wall	W39	#5	3'-0"	8	Wing Wall
W53	#5	3'-0"	5	Wing Wall	W40	#5	3'-0"	8	Wing Wall
W54	#5	3'-0"	5	Wing Wall	W41	#5	3'-0"	8	Wing Wall
W55	#5	3'-0"	5	Wing Wall	W42	#5	3'-0"	8	Wing Wall
W56	#5	3'-0"	5	Wing Wall	W43	#5	3'-0"	8	Wing Wall
W57	#5	3'-0"	5	Wing Wall	W44	#5	3'-0"	8	Wing Wall
W58	#5	3'-0"	5	Wing Wall	W45	#5	3'-0"	8	Wing Wall
W59	#5	3'-0"	5	Wing Wall	W46	#5	3'-0"	8	Wing Wall
W60	#5	3'-0"	5	Wing Wall	W47	#5	3'-0"	8	Wing Wall
W61	#5	3'-0"	5	Wing Wall	W48	#5	3'-0"	8	Wing Wall
W62	#5	3'-0"	5	Wing Wall	W49	#5	3'-0"	8	Wing Wall
W63	#5	3'-0"	5	Wing Wall	W50	#5	3'-0"	8	Wing Wall
W64	#5	3'-0"	5	Wing Wall	W51	#5	3'-0"	8	Wing Wall
W65	#5	3'-0"	5	Wing Wall	W52	#5	3'-0"	8	Wing Wall
W66	#5	3'-0"	5	Wing Wall	W53	#5	3'-0"	8	Wing Wall
W67	#5	3'-0"	5	Wing Wall	W54	#5	3'-0"	8	Wing Wall
W68	#5	3'-0"	5	Wing Wall	W55	#5	3'-0"	8	Wing Wall
AP1	#6	14'-9"	152	Approach Slab	W56	#5	3'-0"	8	Wing Wall
AP2	#6	38'-0"	20	Approach Slab	W57	#5	3'-0"	8	Wing Wall



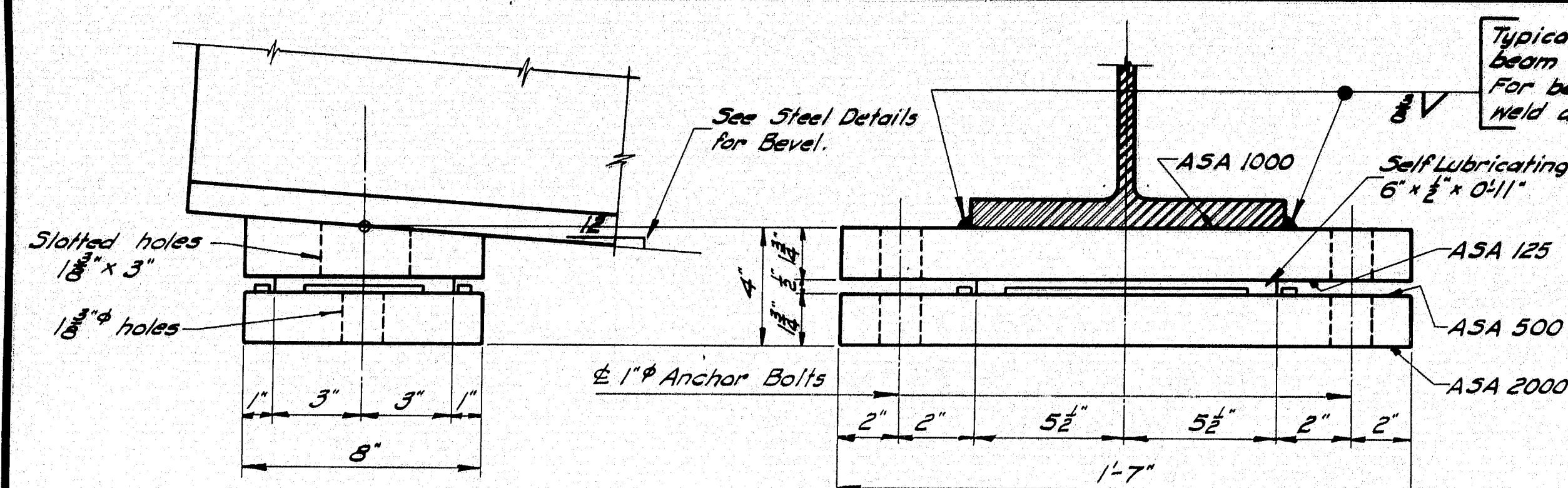
NOTE:  
All reinforcing steel to be intermediate grade.  
All dimensions to & of bars.

DESIGN TRACE - & DET. NOTED CHECK - As Noted	BRIDGE NO. SURVEY - PLOT -
STATE HIGHWAY COMMISSION BRIDGE DIVISION <b>INTERSTATE 95</b> OVER <b>ROUTE 158</b> IN THE TOWN OF <b>SHERMAN</b> <b>AROOSTOOK COUNTY</b> REINFORCING STEEL	
SHEET 10 OF 10 AUGUSTA, MAINE MAY 1966	

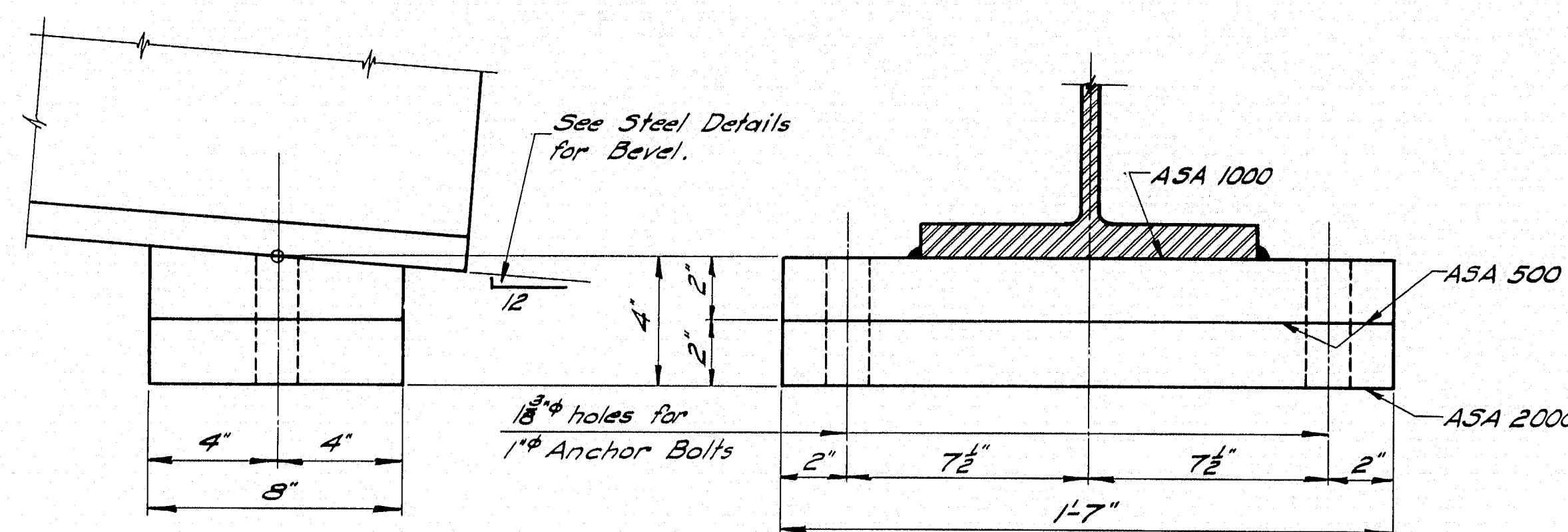
M-2527



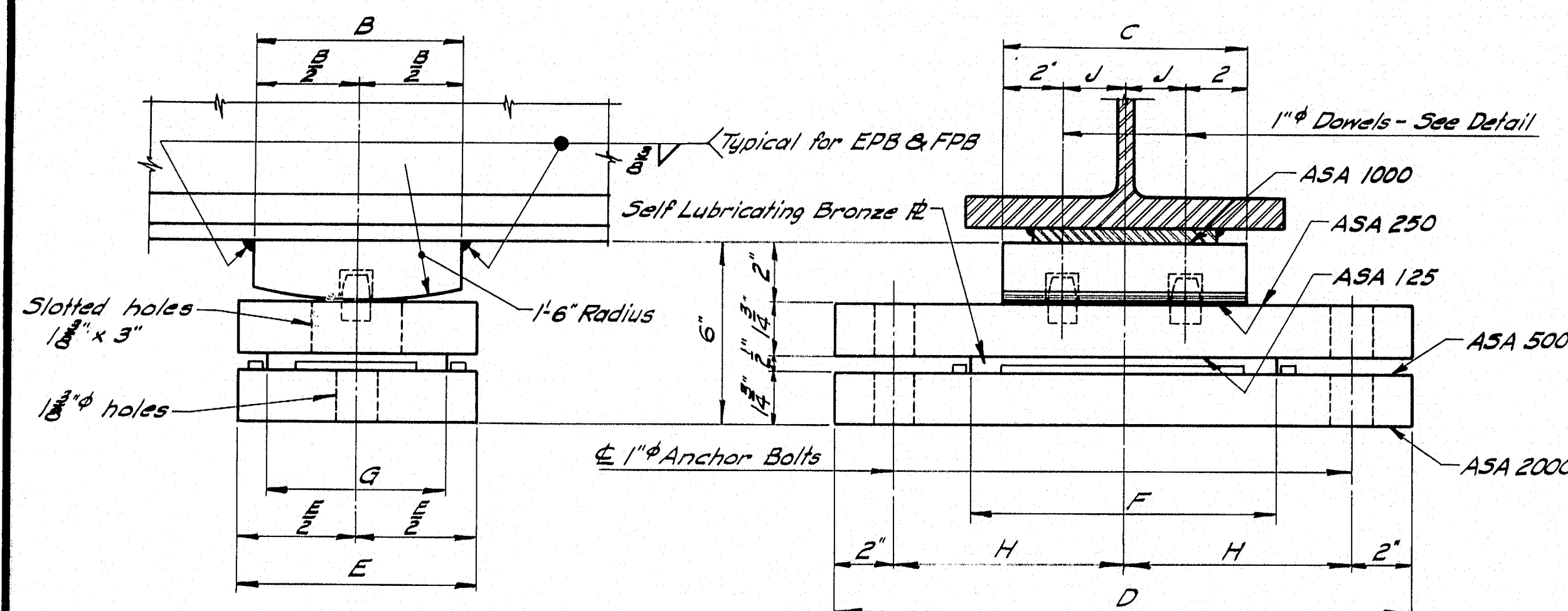




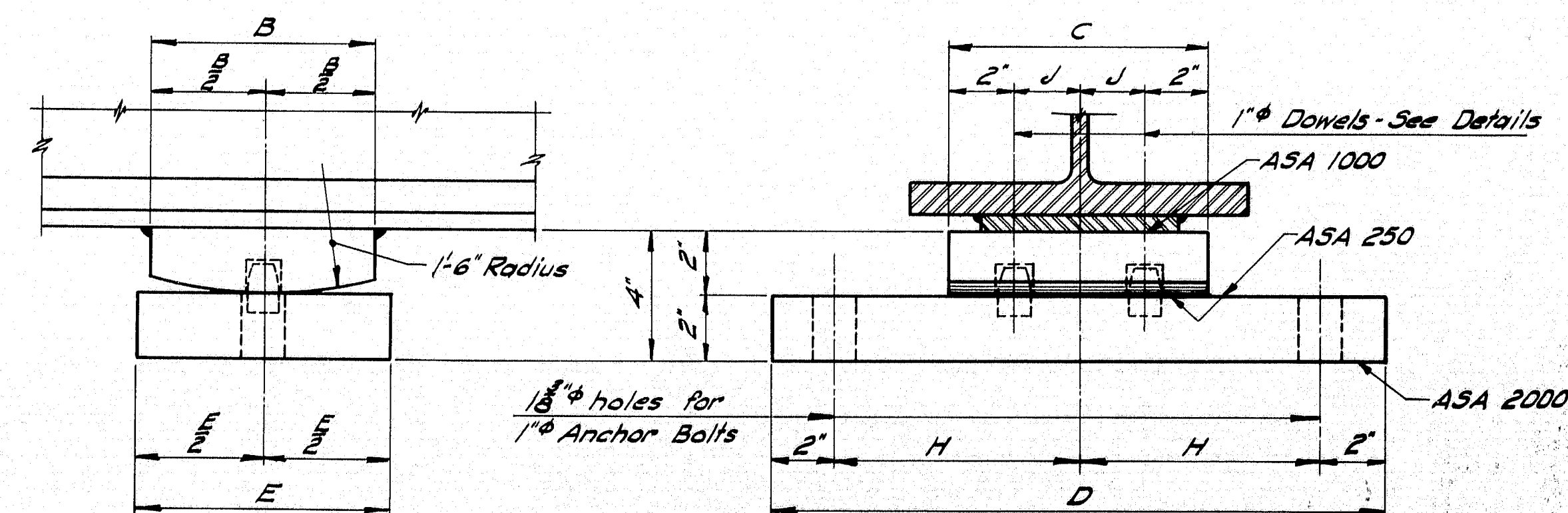
EXPANSION PEDESTAL - EPA



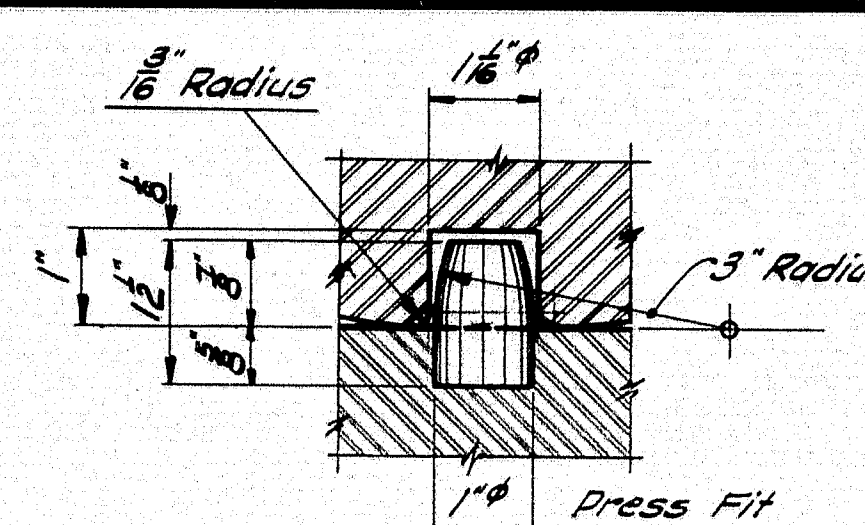
FIXED PEDESTAL - FPA



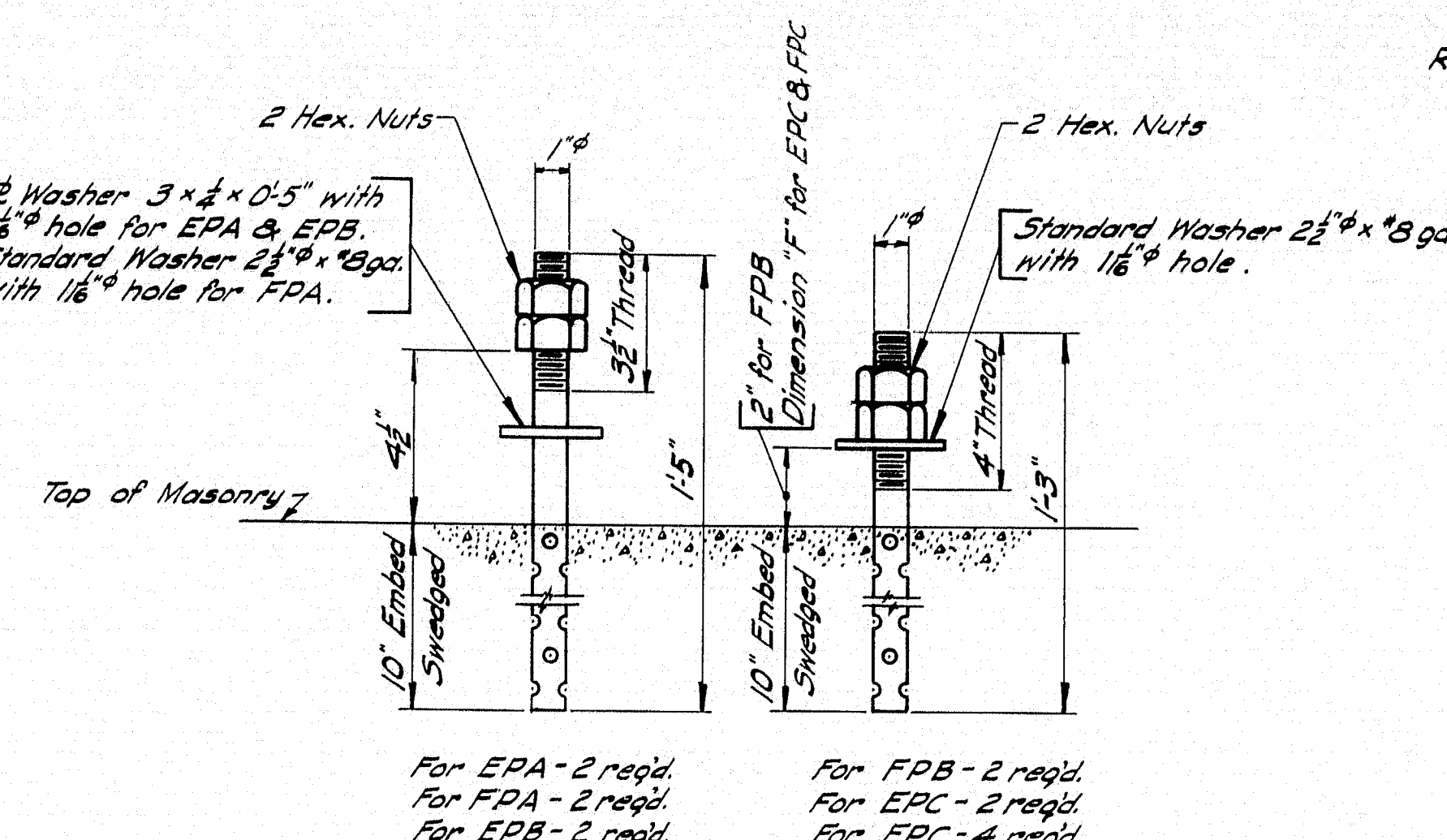
EXPANSION PEDESTAL - EPB



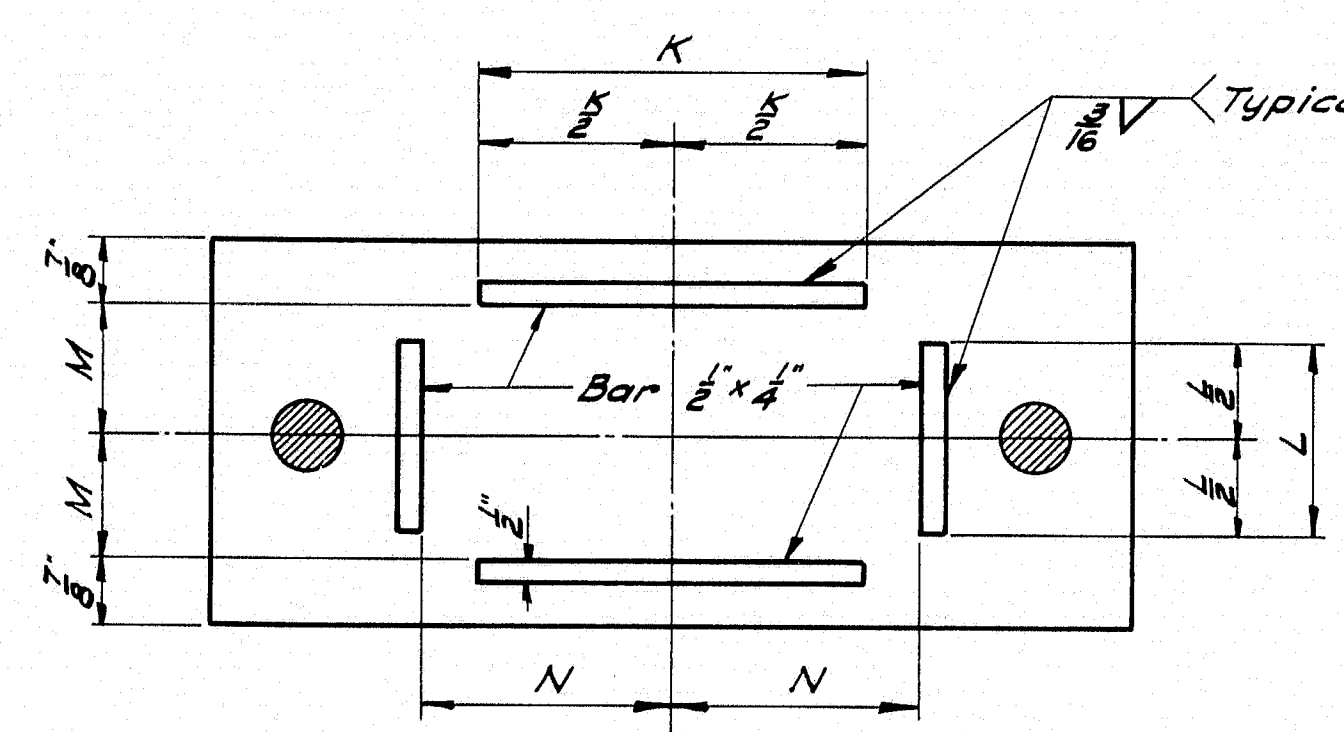
FIXED PEDESTAL - FPB



DOWEL DETAIL

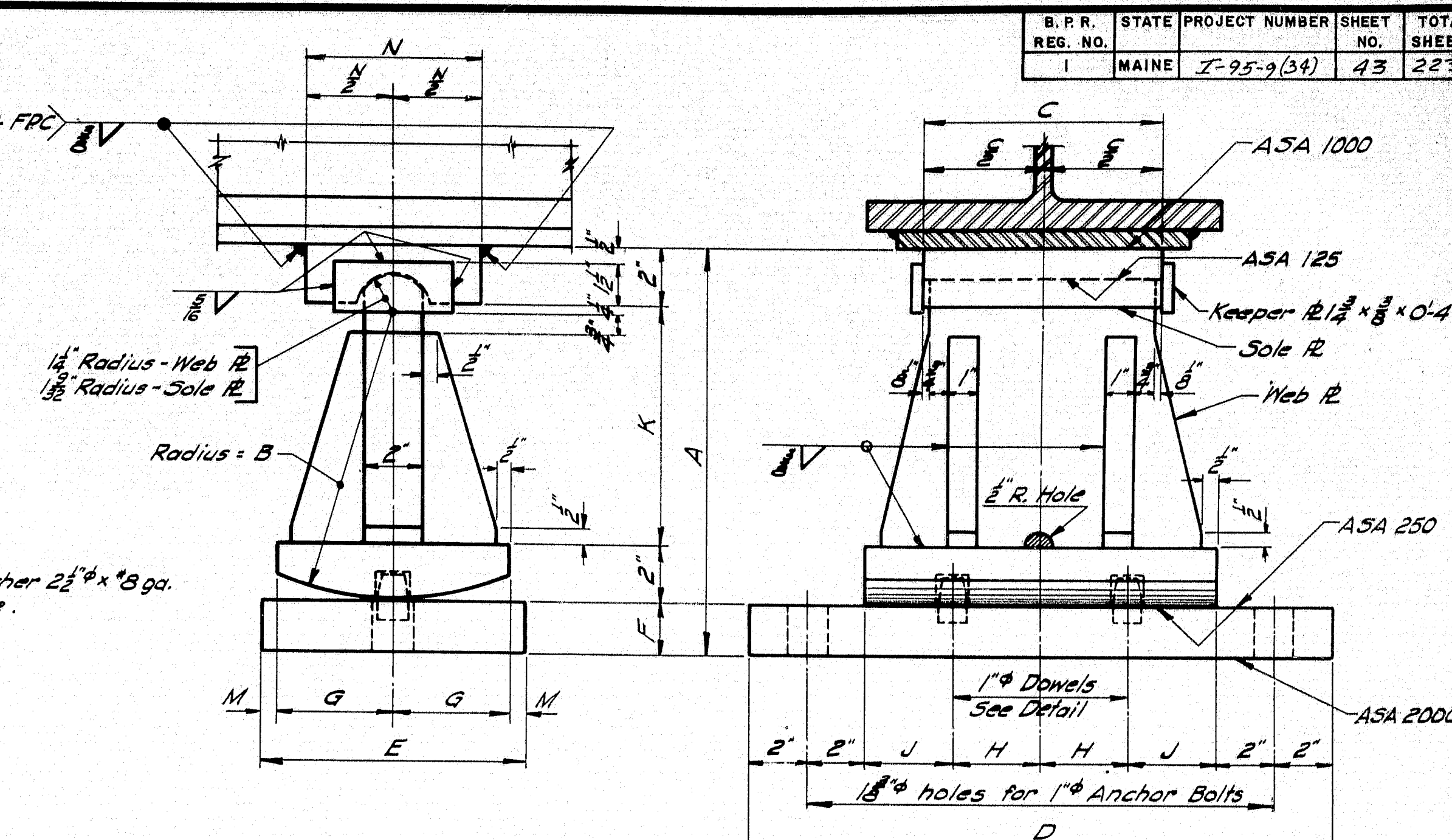


ANCHOR BOLT DETAIL

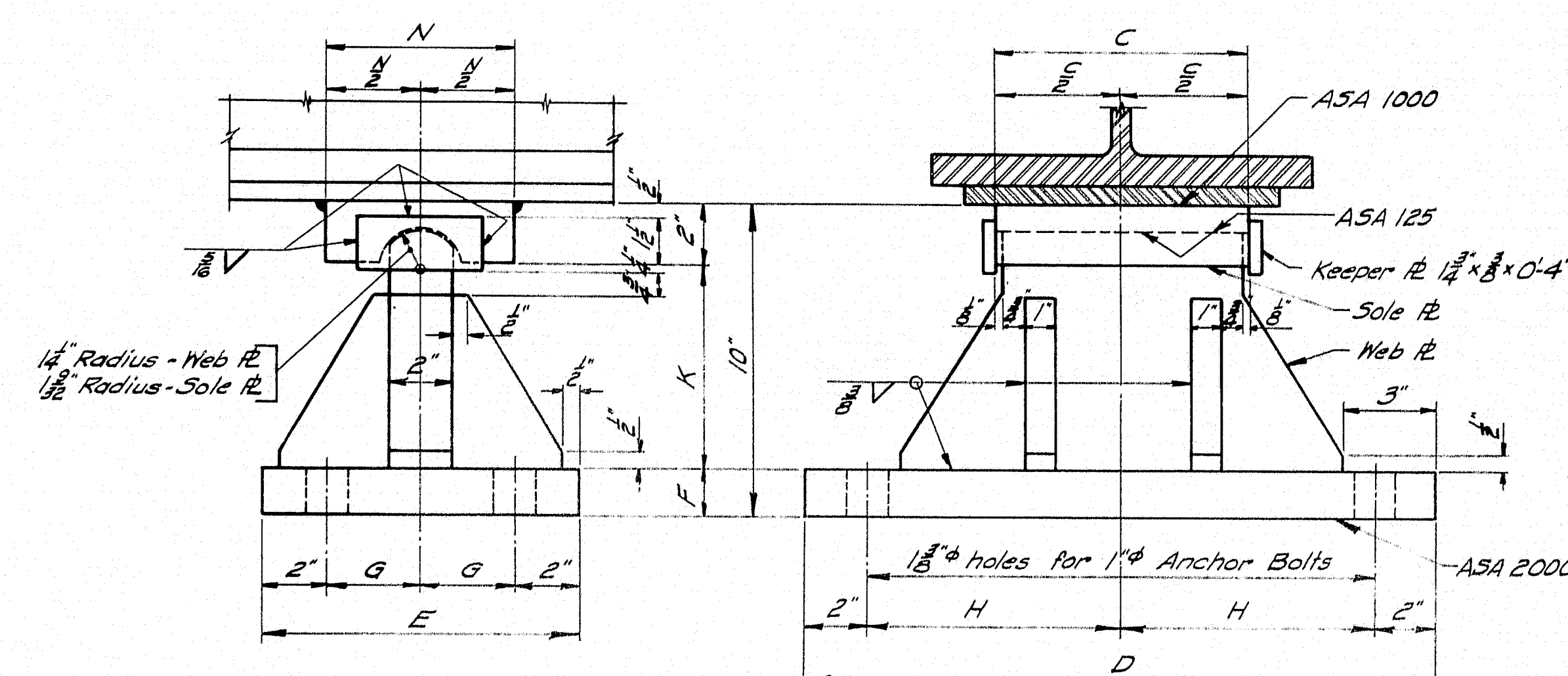


For EPA & EPB  
MASONRY PLATE

PEDESTALS - ALLOWABLE LOADS & DIMENSIONS														
Pedestal	Load	A	B	C	D	E	F	G	H	J	K	L	M	N
EPA	132K	-	-	-	-	-	-	-	-	-	8"	4"	3 1/2"	5 1/2"
FPA	150K	-	-	-	-	-	-	-	-	-	-	-	-	-
EPB-1	120K	-	6"	8"	1-7"	8"	10"	6"	7 1/2"	2"	8"	4"	3 1/2"	5 1/2"
EPB-2	165K	-	7"	10"	1-8"	9"	1-0"	7"	8"	3"	10"	5"	3 1/2"	6 1/2"
EPB-3	224K	-	8"	1-1"	2-0"	10"	1-4"	7"	10"	4 1/2"	1-2"	5"	3 1/2"	6 1/2"
FPB-1	120K	-	6"	8"	1-7"	8"	-	-	7 1/2"	2"	-	-	-	-
FPB-2	165K	-	7"	10"	1-8"	9"	-	-	8"	3"	-	-	-	-
FPB-3	224K	-	8"	1-1"	2-0"	10"	-	-	10"	5"	-	-	-	-
EPC-1	70K	9 1/2"	6"	8"	1-8"	8"	1 1/2"	3 1/2"	3"	3"	4 1/2"	-	1/2"	6"
EPC-2	100K	11 1/2"	8"	8"	1-8"	8"	1 1/2"	3 1/2"	3"	3"	6 1/2"	-	1/2"	6"
EPC-3	130K	1-2"	10"	8"	1-8"	9"	1 1/2"	4"	3"	3"	8 1/2"	-	1/2"	7"
EPC-4	160K	1-2"	10"	8"	1-10"	9"	1 1/2"	4"	3"	3"	8 1/2"	-	1/2"	7"
EPC-5	190K	1-2 1/2"	10"	9"	2-0"	10"	2"	4 1/2"	5"	3"	8 1/2"	-	1/2"	8"
EPC-6	220K	1-4 1/2"	10"	10"	2-0"	10"	2 1/2"	5"	5"	3"	10 1/2"	-	1"	8"
EPC-7	250K	1-4 1/2"	10"	10"	2-2"	10"	2 1/2"	5"	5"	4"	10 1/2"	-	1"	8"
FPC-1	100K	-	-	8"	1-8"	9"	1 1/2"	2 1/2"	8"	-	6 1/2"	-	-	6"
FPC-2	160K	-	-	8"	1-8"	10"	1 1/2"	3"	8"	-	6 1/2"	-	-	7"
FPC-3	190K	-	-	9"	2-0"	10"	1 1/2"	3"	10"	-	6 1/2"	-	-	8"
FPC-4	220K	-	-	10"	2-0"	10"	1 1/2"	4"	10"	-	6 1/2"	-	-	8"
FPC-5	250K	-	-	10"	2-0"	10"	2"	4"	10"	-	6"	-	-	8"



EXPANSION PEDESTAL - EPC



FIXED PEDESTAL - FPC

NOTE: At the location of bearing pedestals the concrete bridge seats shall be dressed one inch larger all around than size of masonry plates and to exact elevations shown on the plans. If dressed areas are below the surface of the surrounding bridge seat a small channel shall be cut to the edge of the bridge seat for drainage where required by the Engineer. Channels shall have a min. width of 2" and min. slope of 1/8 inch per foot. No separate payment for this work will be made as it shall be considered incidental to contract items.

DESIGN SPECIFICATIONS

A.A.S.H.O., Standard Specifications for Highway Bridges, 1961, with Interim Specifications, 1961 & 1962

A.S.T.M. STEEL CLASSIFICATION

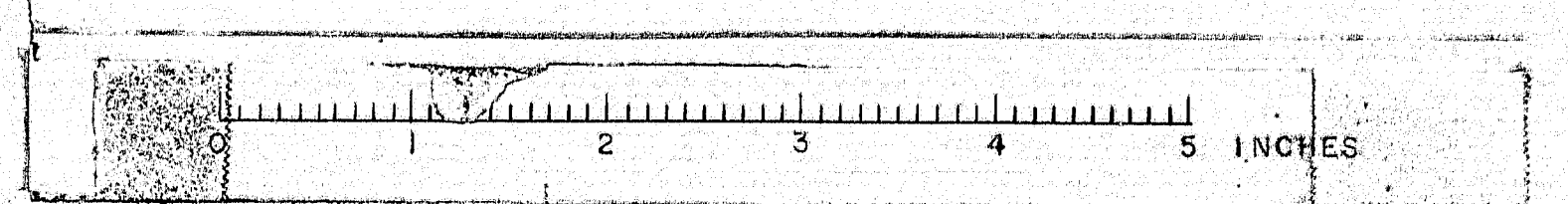
Anchor Bolts - A7, A36, or A307  
All other - A36.

MAINE STATE HIGHWAY COMMISSION  
AUGUSTA, MAINE

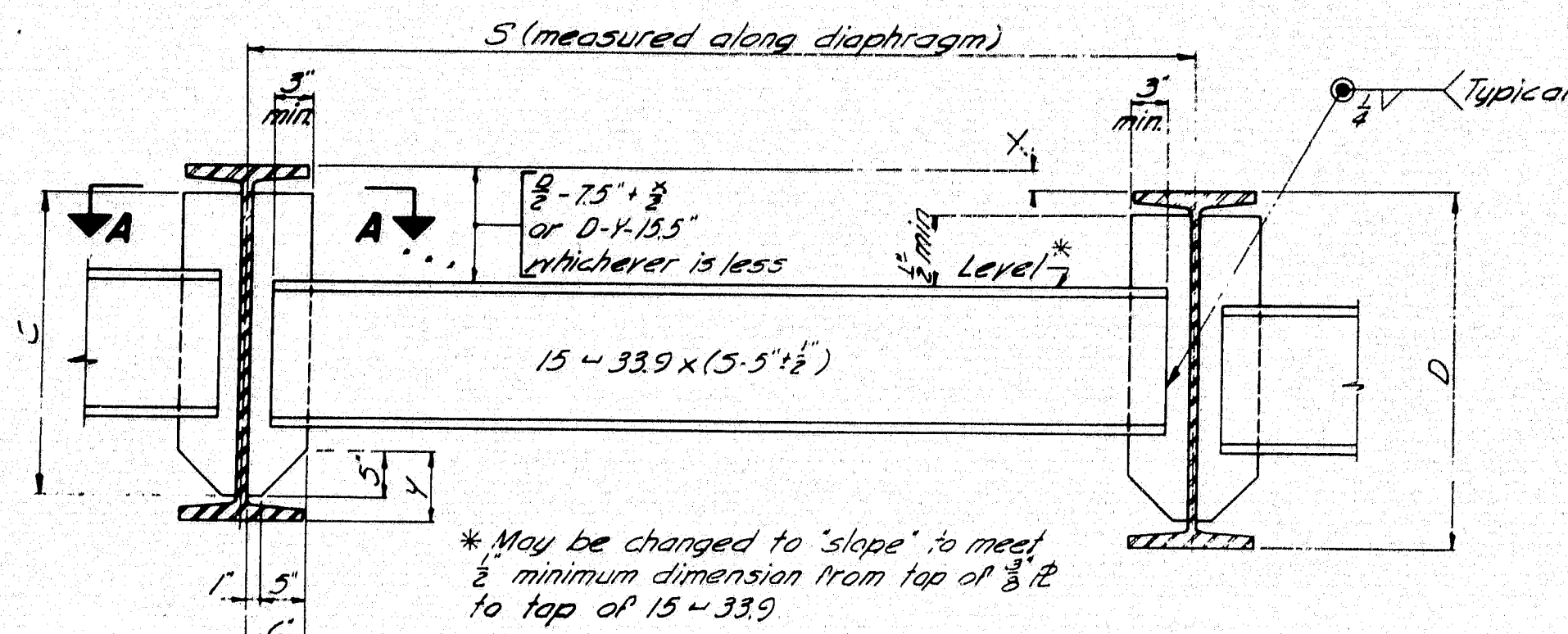
STANDARD DETAILS  
(BD 101-64)

BEARING PEDESTALS

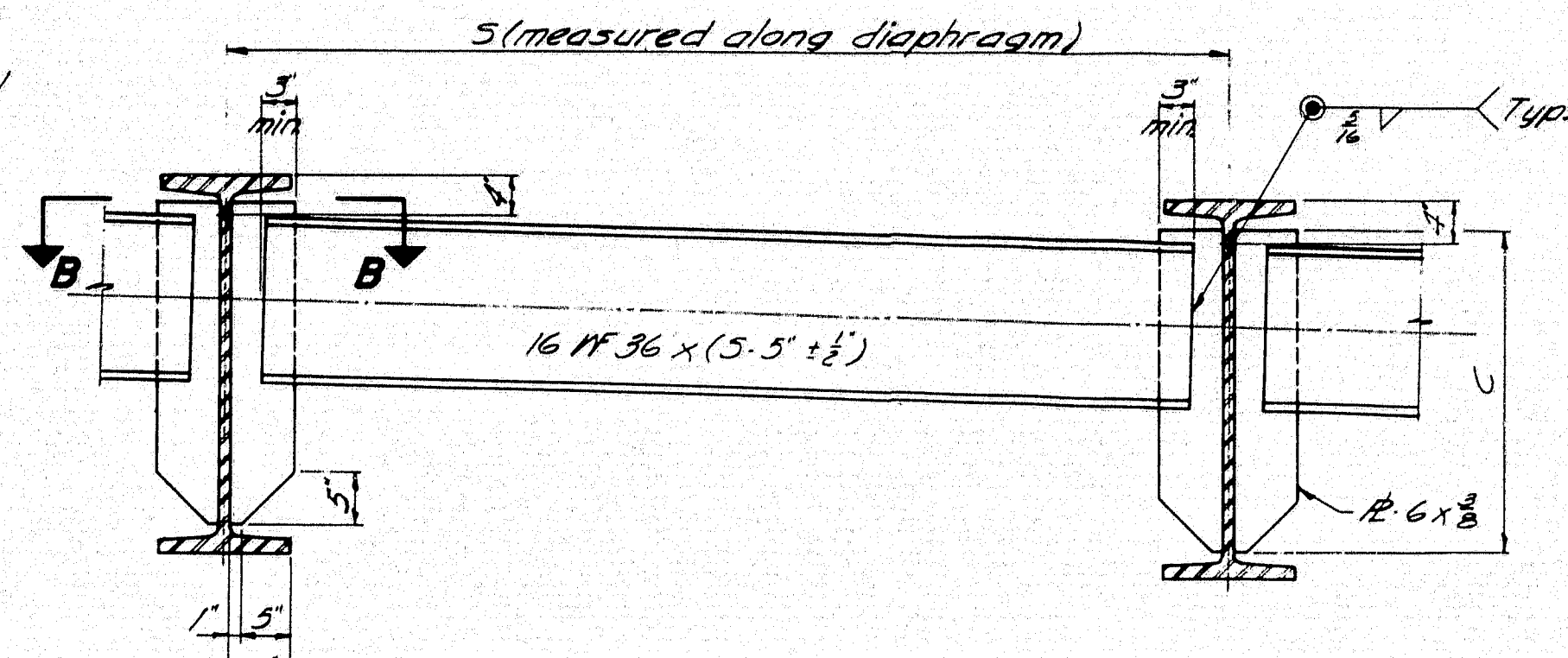
JANUARY, 1964



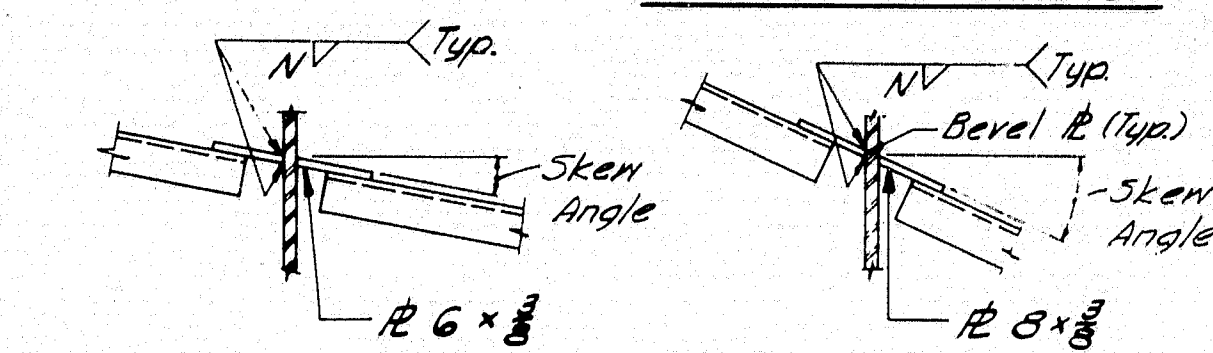




**TYPE A DIAPHRAGM**



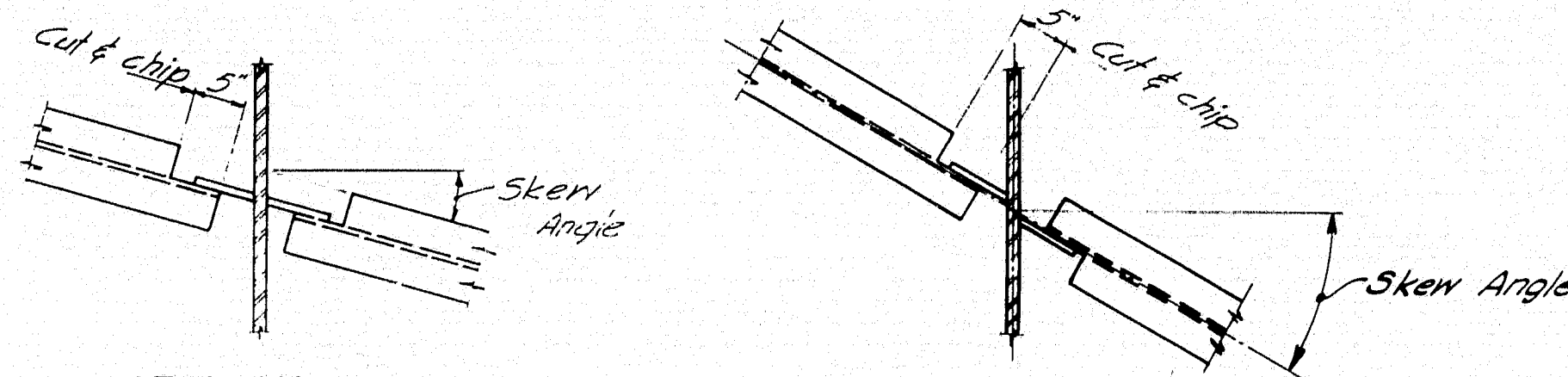
**TYPE B DIAPHRAGM**  
Welding 6x3/8 plates to web same as for Type A Diaphragm.



**SECTION A-A**  
Skew Angle 0° to 10°-00'

**SECTION A-A**  
Skew Angle over 10°-00' to 20°-00'

FILLET WELD SIZE "N" & DIMENSION "C" FOR DIAPHRAGM PLATES		
BEAM	C	N
27 WF 84 to 114 incl.	1-11"	1/2"
30 WF 99 to 132 incl.	2-2"	3/4"
33 WF 118 to 152 incl.	2-5"	1"
36 WF 135 to 194 incl.	2-7"	1 1/4"
36 WF 230 to 300 incl.	2-6"	1 1/2"

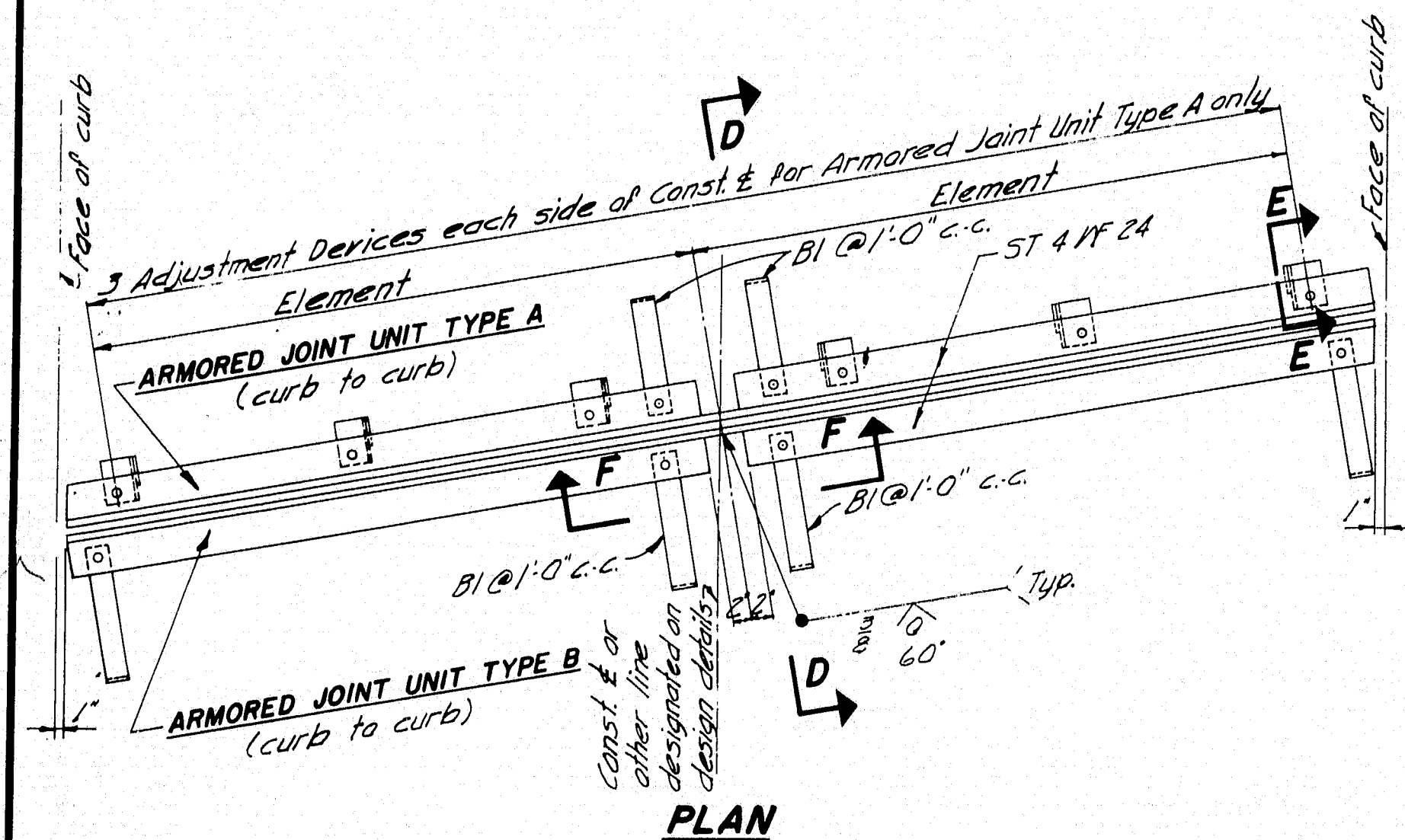


**SECTION B-B**  
Skew Angle 0° to 10°-00'

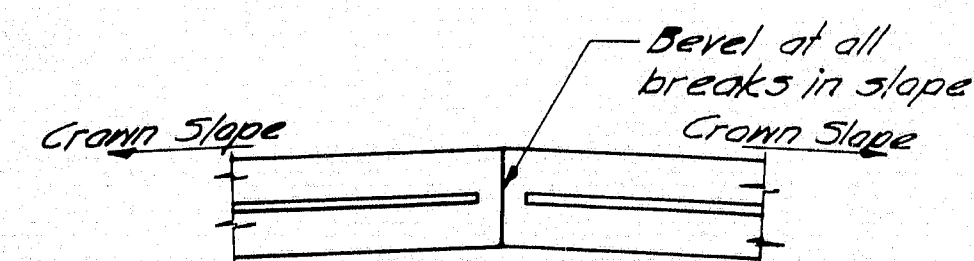
**SECTION B-B**  
Skew Angle over 10°-00'

**NOTE**  
See design details for diaphragm type, location and skew.

## DIAPHRAGMS



**PLAN**



**SECTION F-F**

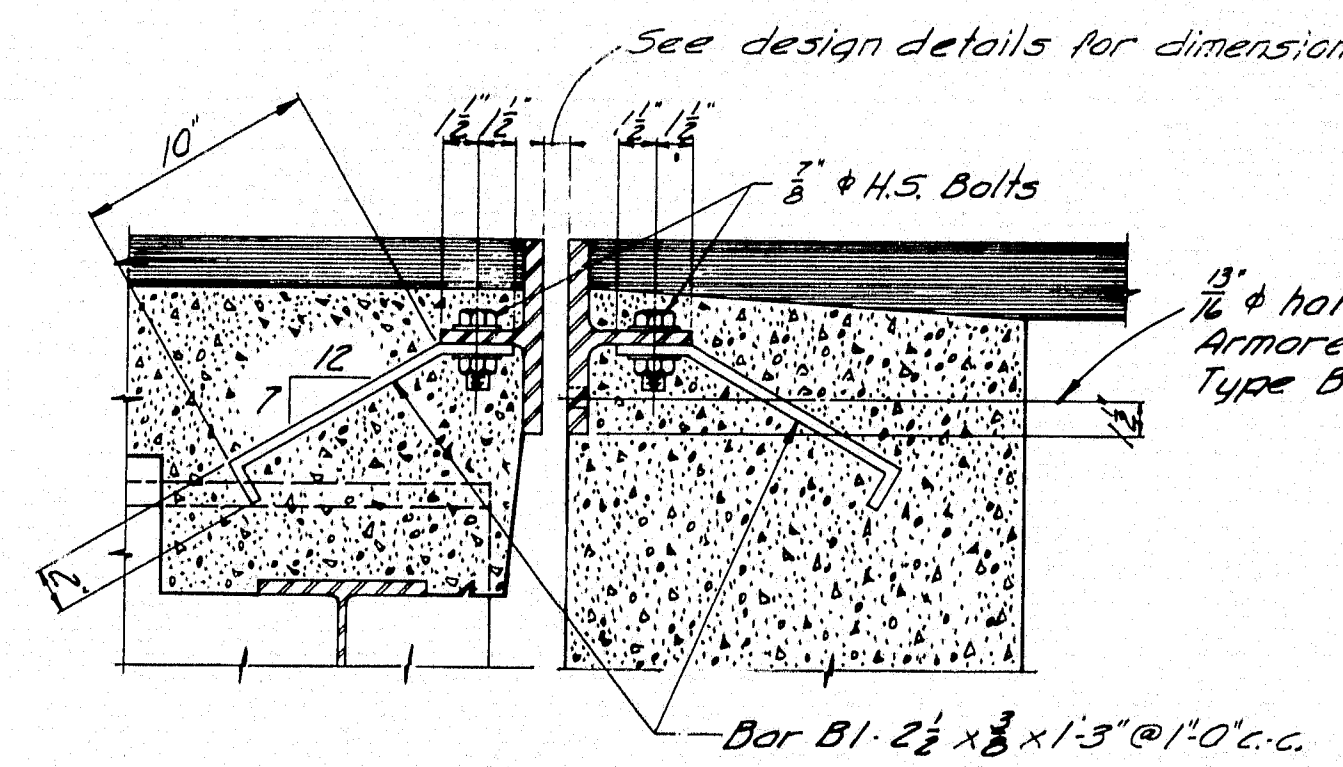
Note: See design details for const. & to curb dimensions, skew, crown slope, slab thickness, other dimensions necessary to complete the fabrication details, and location.

## NOTE

1. Type A Armored Joint Units are intended to be used for attachment to superstructures. Type B Armored Joint Units are intended to be used for attachment to abutments. At armored joints over piers, two (2) Type A Armored Joint Units shall be used.
2. If more elements than the two shown in the "Plan" are required by the design details, there shall be three adjustment devices for each element for Armored Joint Unit Type A and the elements of both units shall be field welded together in the same manner as shown in the "Plan".
3. Armored Joints to be paid for as Structural Steel.

## ARMORED JOINT

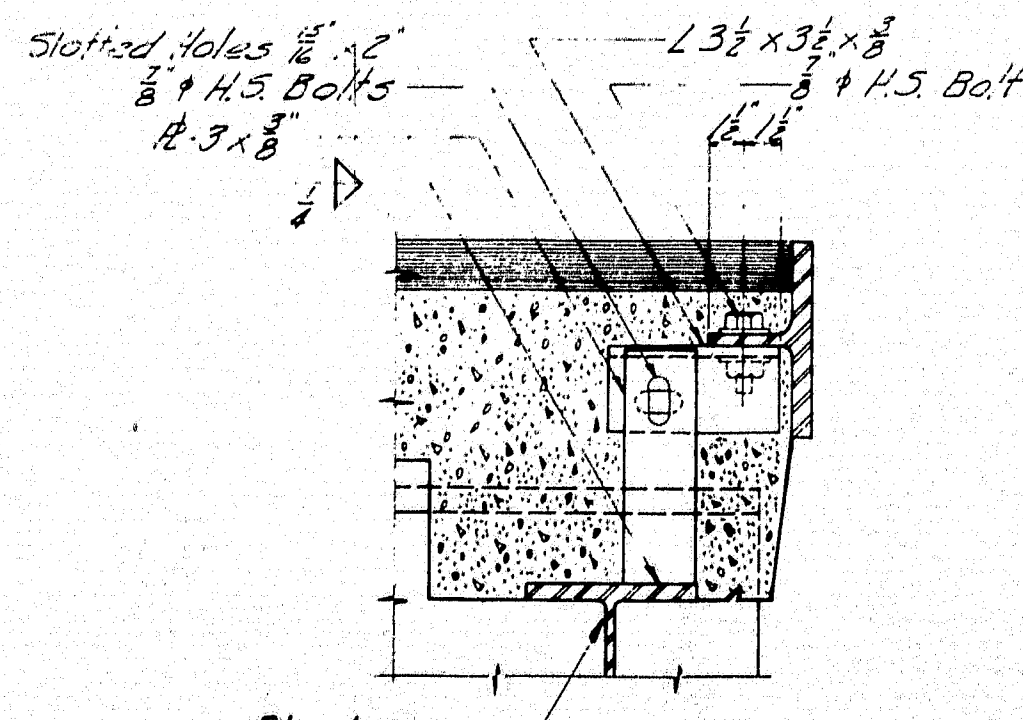
An armored joint consists of two armored joint units. See note 1.



**ARMORED JOINT UNIT TYPE A**

**ARMORED JOINT UNIT TYPE B**

**SECTION D-D**

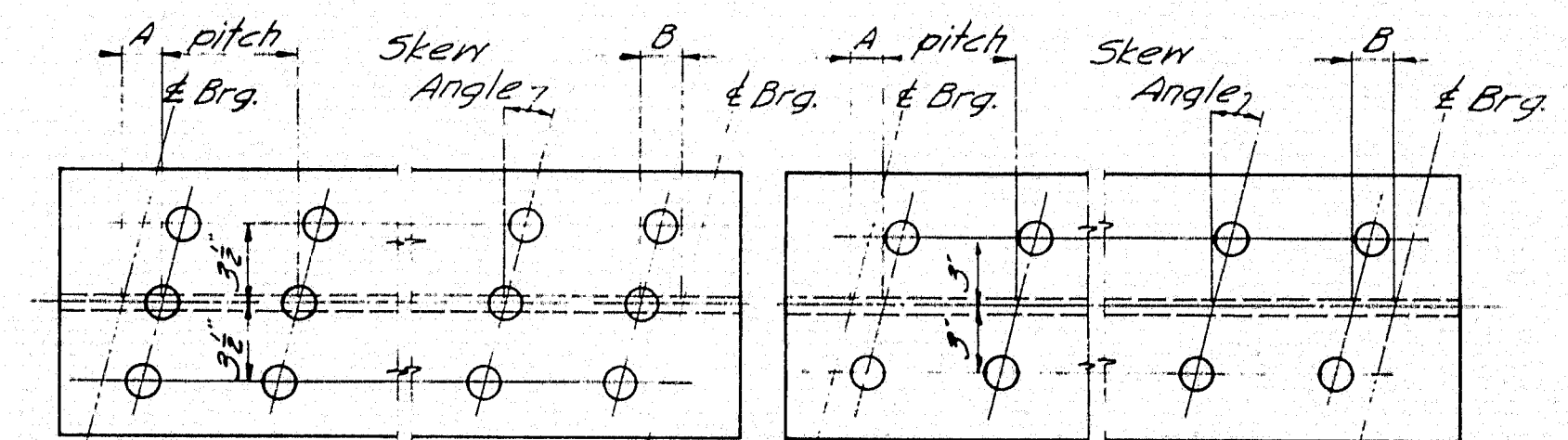


**SECTION E-E**

Shaving Adjustment Device Armored Joint Unit Type A only - After Unit is in final position weld 3/8" to angle with 1/2" fillet

## SHEAR CONNECTOR NOTE

The connectors may be either steel studs or spirally formed bars. At the request of the Contractor a plan for using spirally formed bars will be provided.



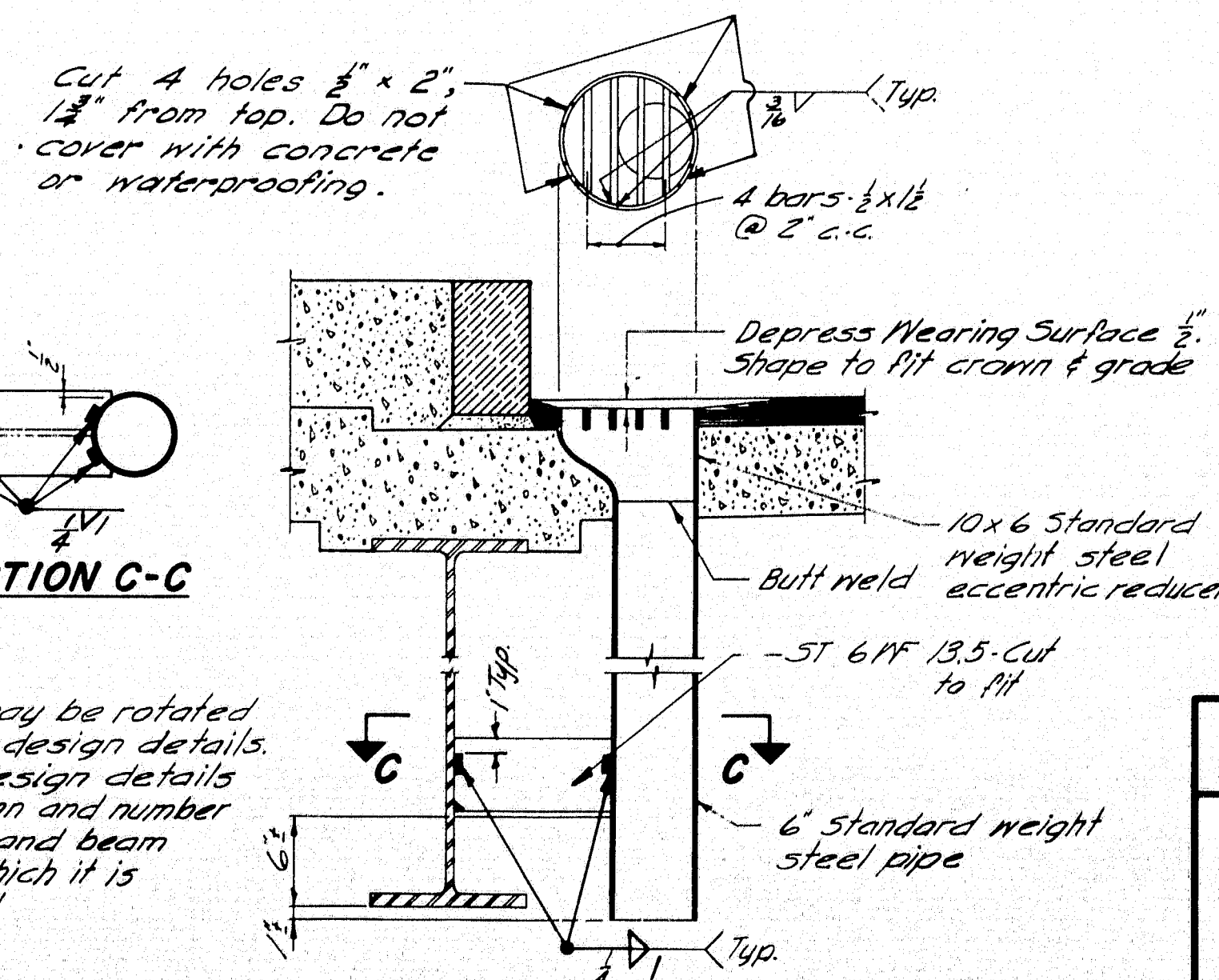
**TRIPLE STUDS**

**DOUBLE STUDS**

## NOTE

1. Studs shall be granular or solid flux filled and automatically end welded to top flange in the shop or field.
2. See the design details for Dimensions "A" & "B", stud pitch and skew angle for studs.

## SHEAR CONNECTORS



**SECTION C-C**

## NOTE

1. Drain may be rotated 180°. See design details.
2. See design details for location and number of drains and beam size to which it is connected.

**DRAIN**

## GENERAL NOTE

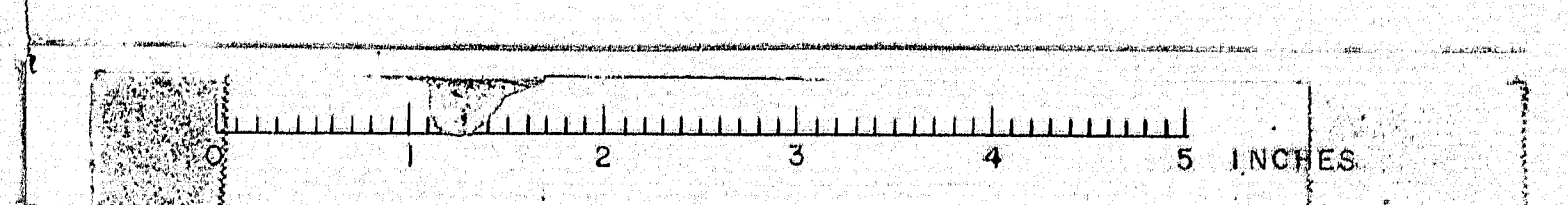
Use only those items called for on design details. In case of conflict between these Standard Details and the design details, the requirements of the design details shall be followed.

MAINE STATE HIGHWAY COMMISSION  
AUGUSTA, MAINE

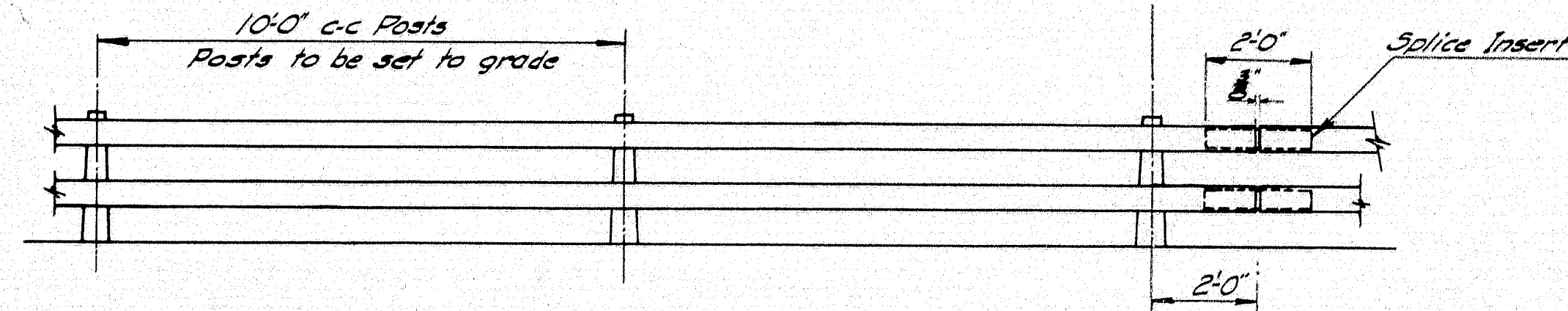
**STANDARD DETAILS**  
(BD 104-66)  
**DIAPHRAGMS, ARMORED JOINT, SHEAR CONNECTORS, DRAIN**

SEPTEMBER 1966

M-2527B

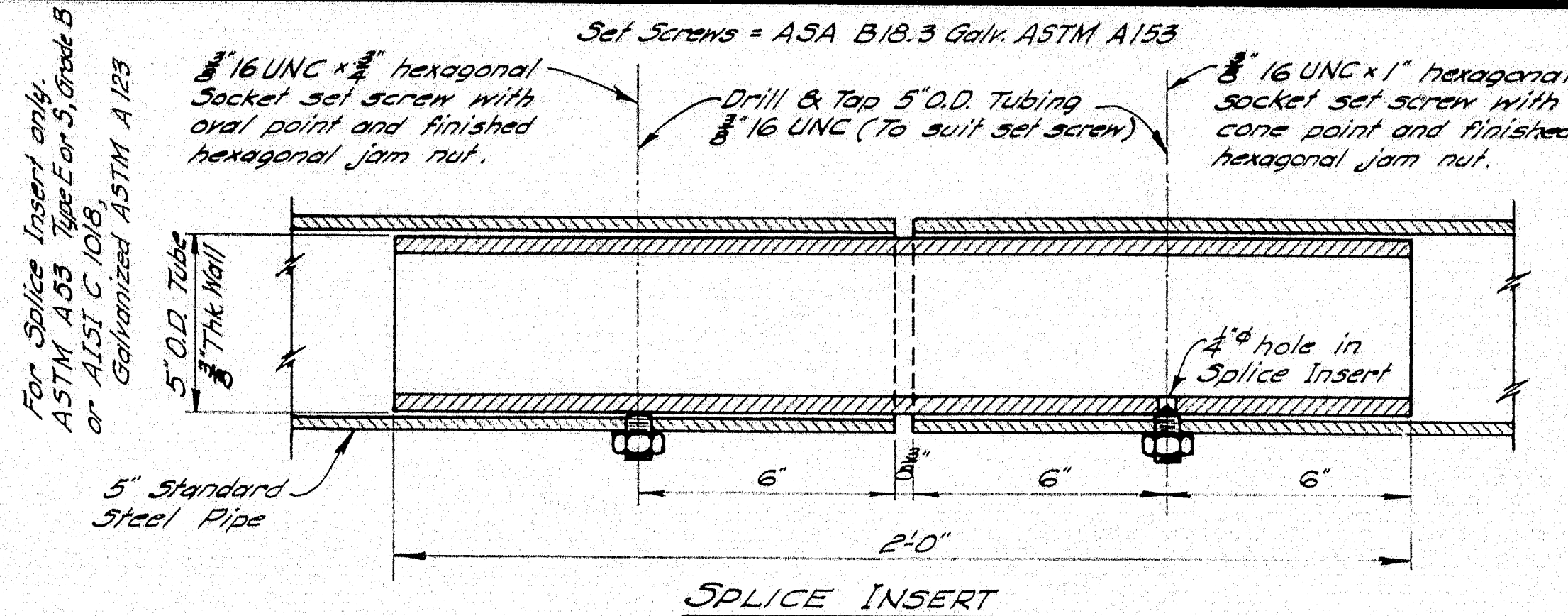




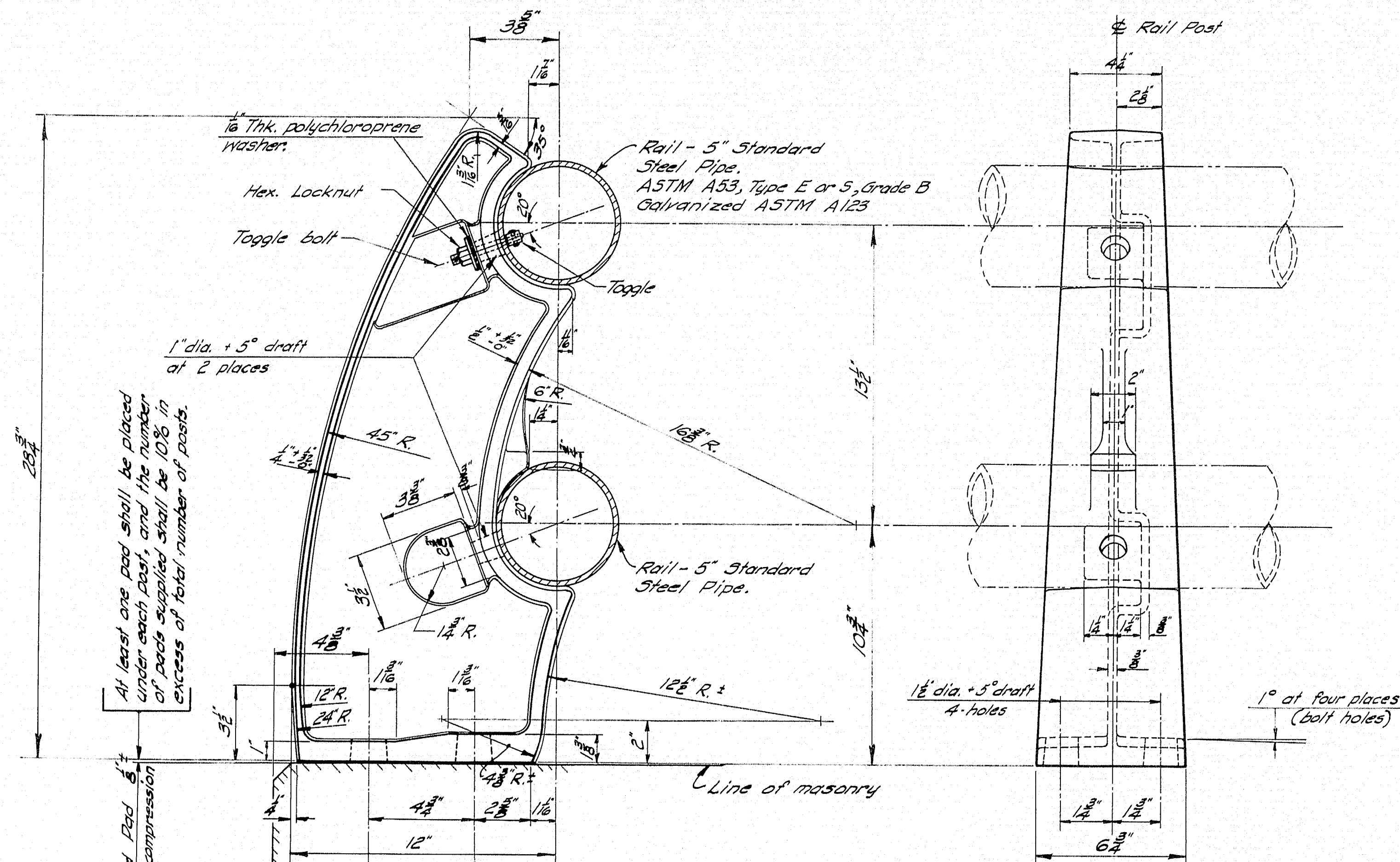


**RAIL ELEVATION**

Lengths of rail shall be attached to a minimum of (4) four rail posts, whenever possible, and in any case never less than (2) two.

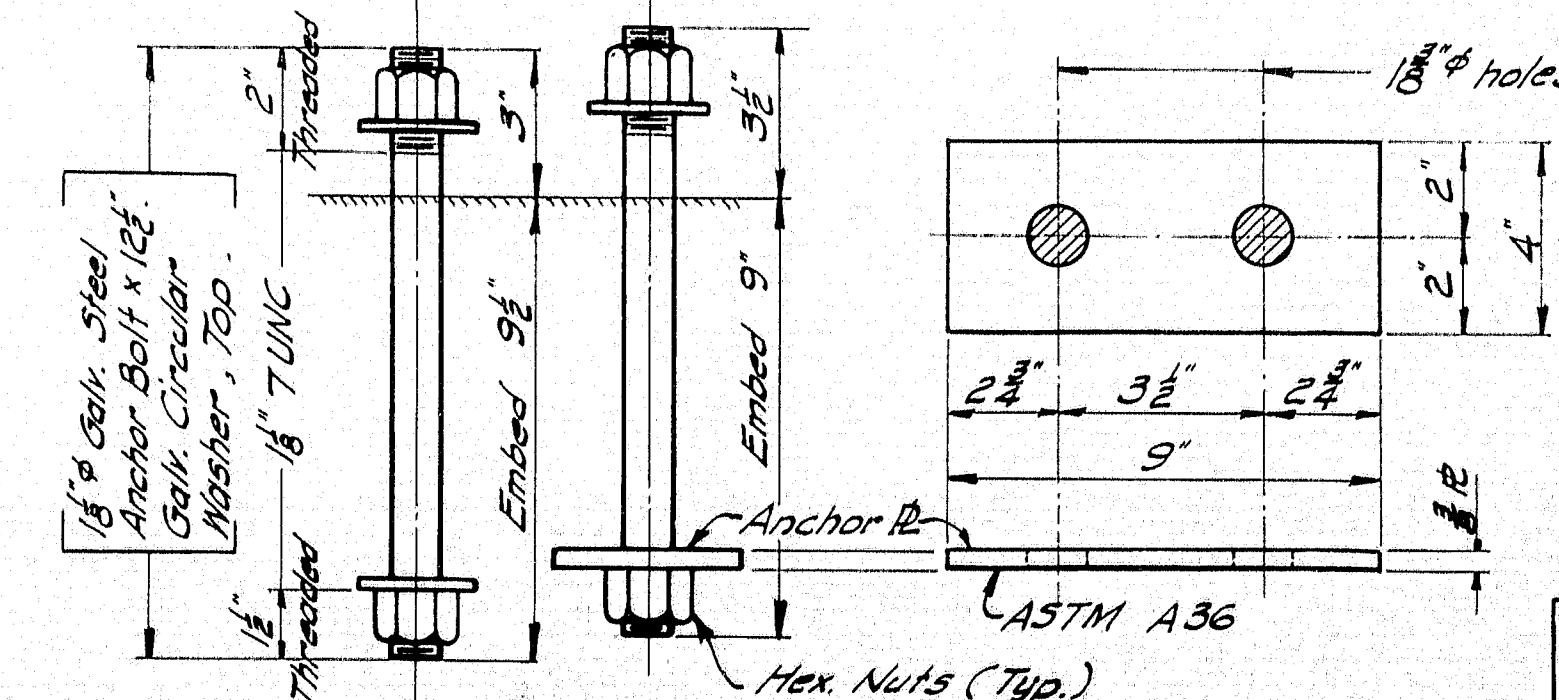


**SPICE INSERT**



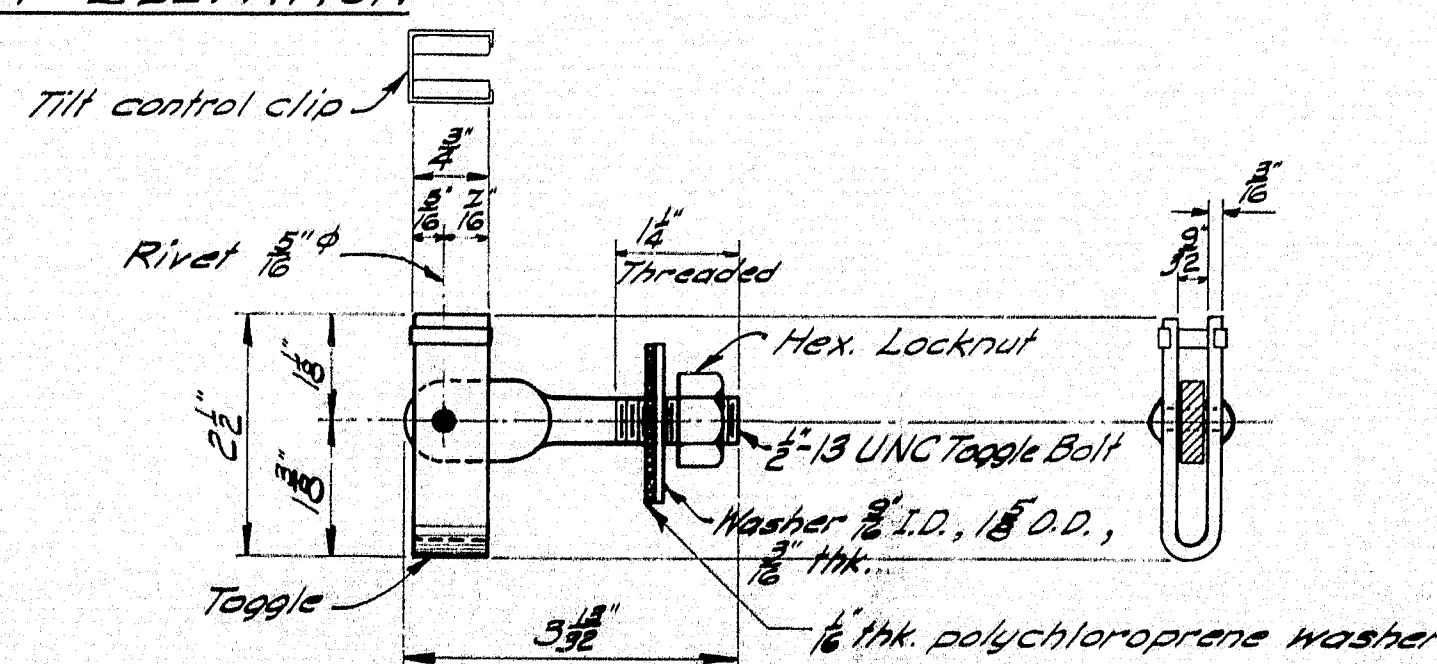
**RAIL POST**  
ASTM A27, Grade 65-35, Galvanized ASTM A153

**FRONT ELEVATION**



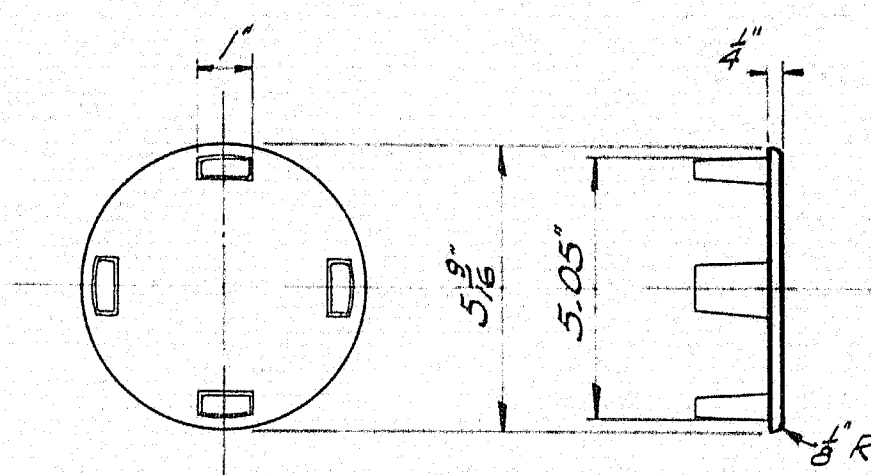
**RAIL POST ANCHORAGE**  
Anchor Bolts, Nuts, & Circular Washers = ASTM A325  
Anchor Bolts, Nuts & Circular Washers at Top, (Galv.) ASTM A153

REQUIRED PER ANCHORAGE  
4 - Anchor Bolts  
6 - Circular Washers  
1 - Anchor Nut

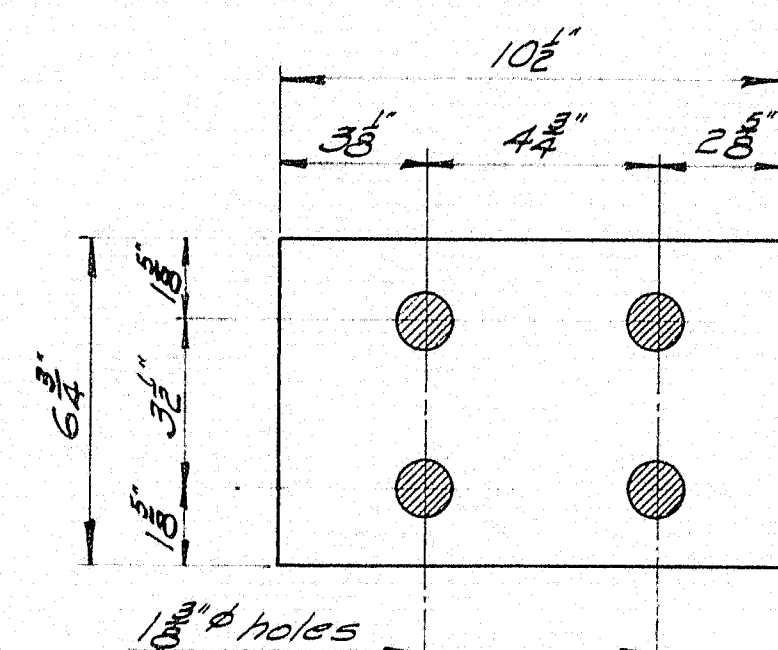


**TOGGLE BOLT ASSEMBLY**  
Cadmium Plate metal parts ASTM A165

Required 2-per post



**RAIL CAP**  
ASTM A27, Grade 65-35, Galv. ASTM A153



**PREFORMED PAD**

See Subsection 713.03 Standard Specifications  
Revision of June 1965 for pad and fabric washers.

Required 1-Pad per post

**DESIGN SPECIFICATIONS**  
A.A.S.H.O. Interim Specifications  
Int. I (64)

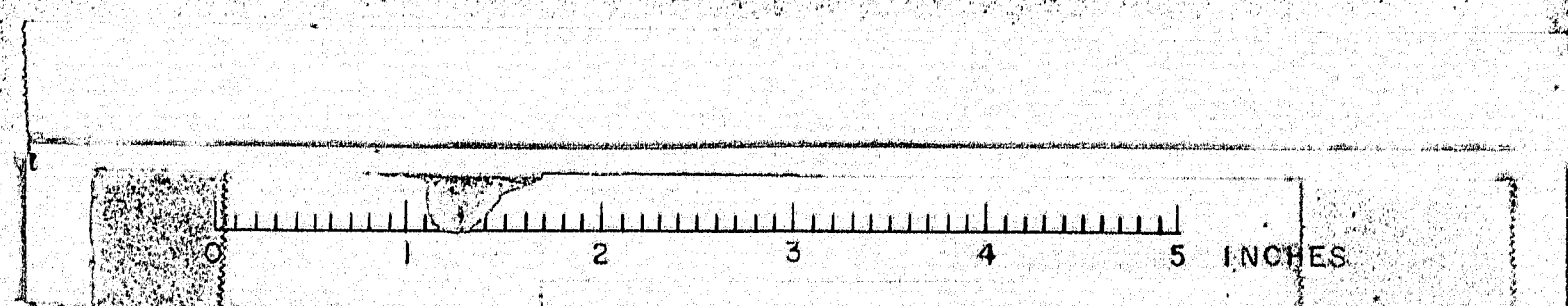
- Toggle = ASTM A303, 1015 H.R. Steel.
- Rivet = ASTM A195, 1035 C.R. Steel, Heat Treated
- Toggle Bolt = ASTM A354, 1335 C.R. Steel, Heat Treated RC 32-38.
- Washer = ASTM A36 Steel
- Hex. Locknut = Finished Hexagonal Locknut Prevailing Torque Type Steel Grade Cor D, Industrial Fasteners Institute.

MAINE STATE HIGHWAY COMMISSION  
AUGUSTA, MAINE

**STANDARD DETAILS**  
(BD 107 - 65)  
**STEEL RAILING**  
(2-BAR PIPE RAIL)  
CAST POST  
OCTOBER 1965

Revised - Toggle Bolt Sept. 1966

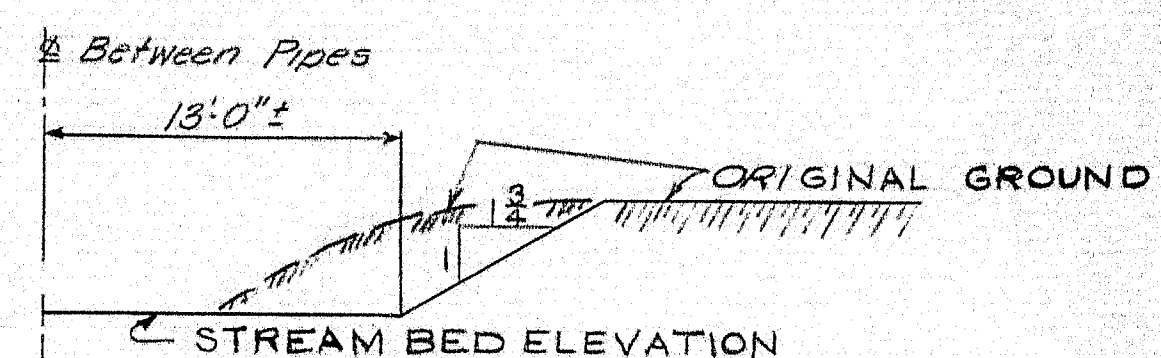
M-2529C-I-95 over Beredick Road - SHERMAN



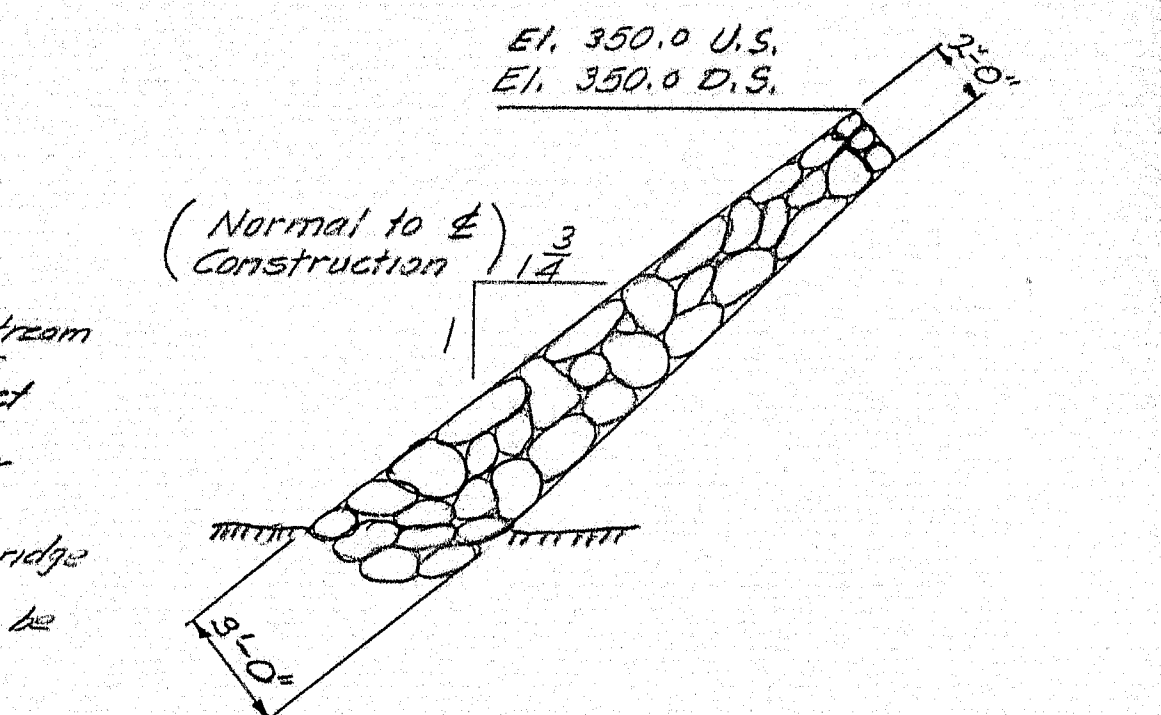




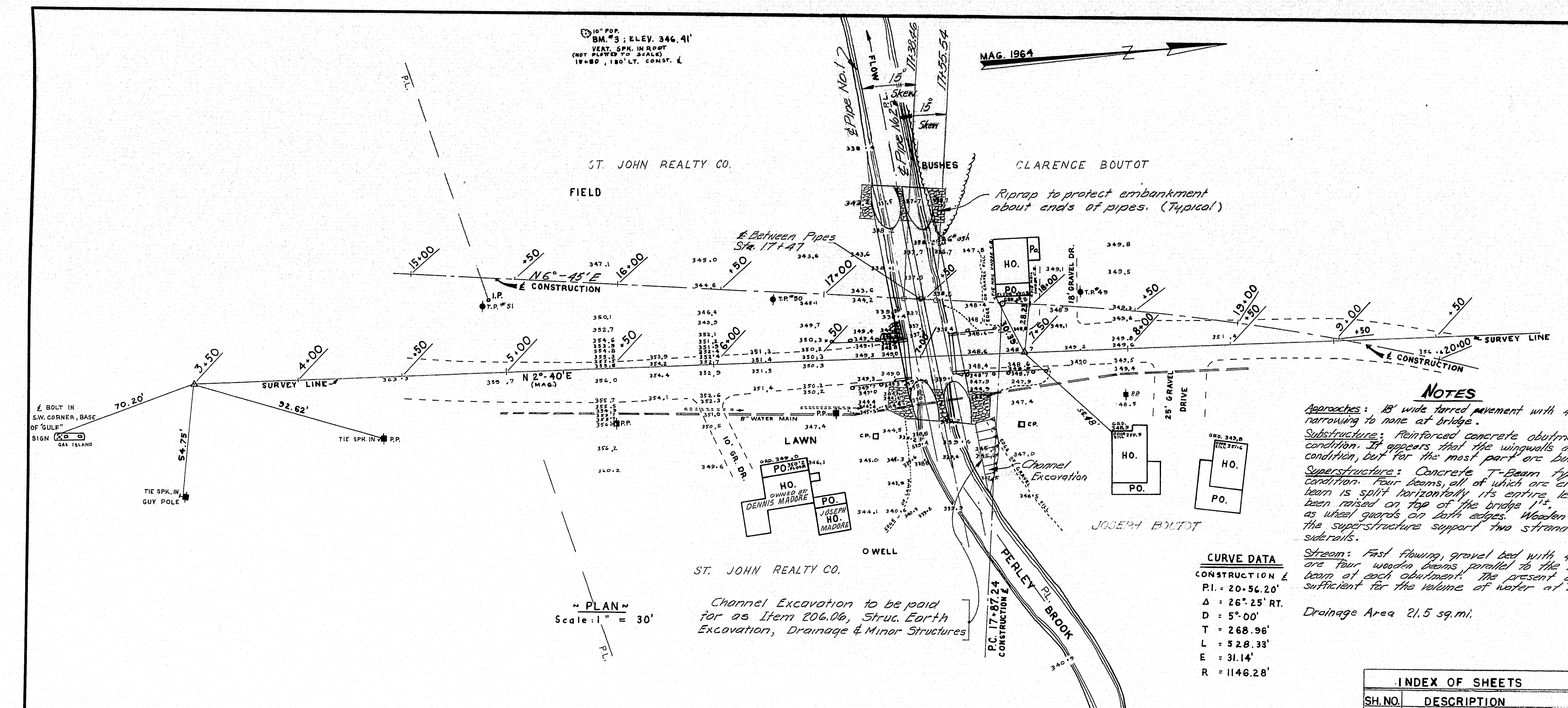




TYPICAL CHANNEL SECTION



TYPICAL RIPRAP SECTION



**NOTES**  
Approaches: 18' wide tarred pavement with 4' shoulders narrowing to none at bridge.  
Substructure: Reinforced concrete abutments in very poor condition. It appears that the wingwalls are in very poor condition, but for the most part are buried.  
Superstructure: Concrete T-Beam type in very poor condition. Four beams, all of which are cracked. The downstream beam is split horizontally its entire length. The beam has been raised on top of the bridge piers. Wooden beams act as wheel guards on both edges. Wooden posts bolted to the superstructure support two strands wire cable for subrails.  
Stream: Fast flowing, gravel bed with 4" rocks. Under bridge are four wooden beams parallel to the road with a cross beam at each abutment. The present opening appears to be sufficient for the volume of water at high periods.  
Drainage Area 21.5 sq. mi.

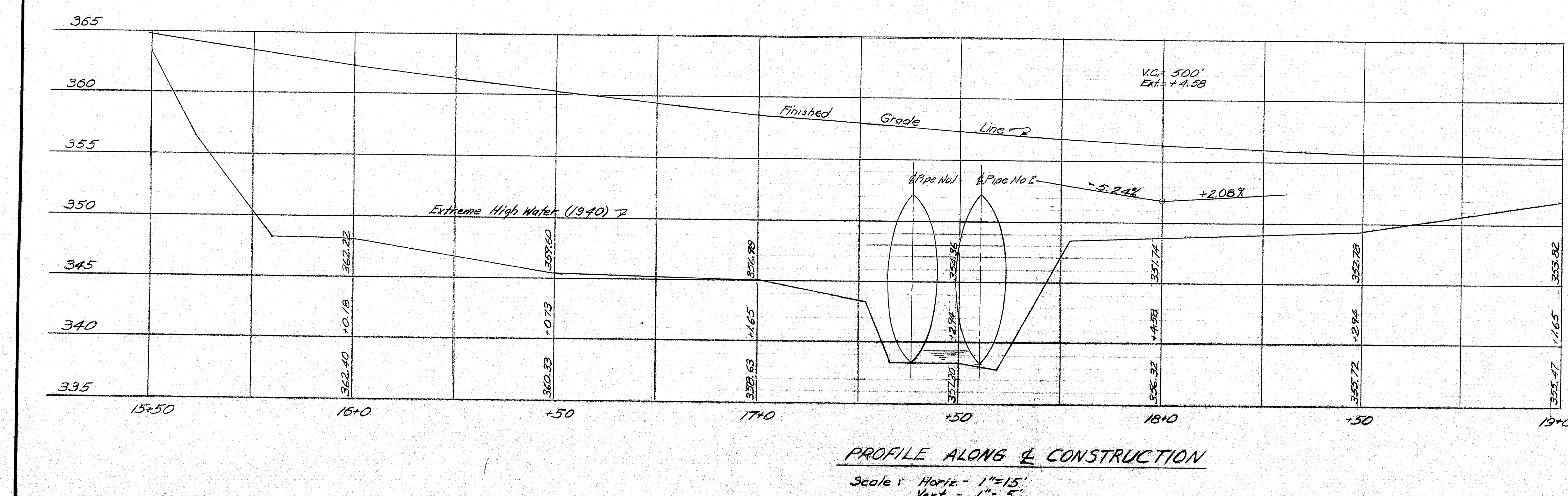
**CURVE DATA**  
CONSTRUCTION &  
P.I. = 20+56.20'  
Δ = 26° 25' RT.  
D = 5° 00'  
T = 268.96'  
L = 528.33'  
E = 31.14'  
R = 1146.28'

**INDEX OF SHEETS**

SH. NO.	DESCRIPTION
1	SURVEY
2	STRUCTURAL PLATE PIPES
3	FOUNDATION SURVEY

**ESTIMATE OF QUANTITIES - PERLEY BROOK BRIDGE FORT KENT**

DESCRIPTION	QUANTITY
Structural Earth Excavation, Drainage and Minor Structures	225 cy.
Structural Rock Excavation, Drainage and Minor Structures	60 cy.
Granular Borrow	1,200 cy.
Removal of Existing Superstructure (Property of Contractor)	
Perley Brook Bridge, 156" Structural Plate Pipes	Lump Sum
Hand Laid Riprap	65 cy.



PROFILE ALONG & CONSTRUCTION  
Scale: Horiz. - 1" = 15'  
Vert. - 1" = 5'

DESIGN - R. GRAY  
TRACE - L. ROBERTS  
CHECK - L. ROBERTS

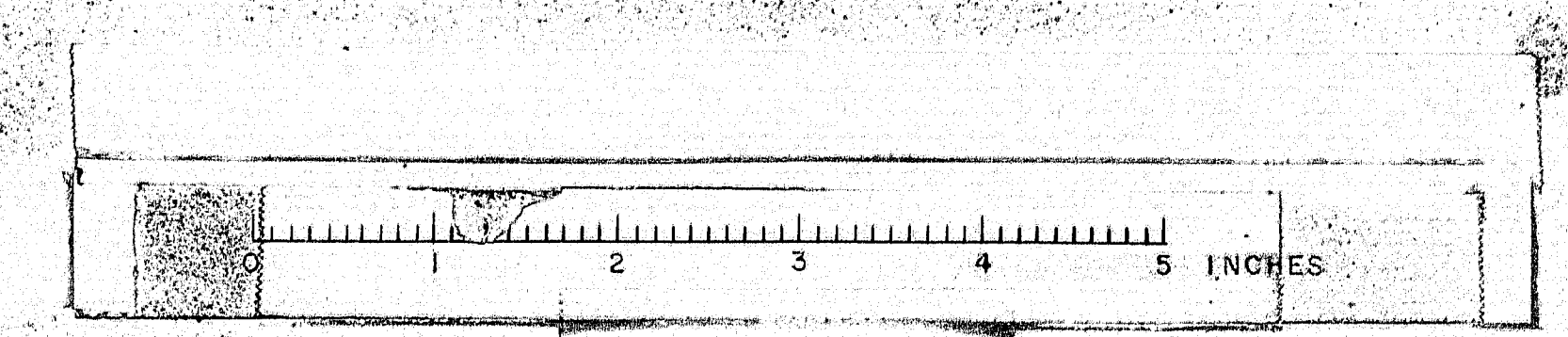
BRIDGE NO. 3468  
SURVEY - HARDISON  
PLOT - LIBBY

STATE HIGHWAY COMMISSION  
BRIDGE DIVISION

**PERLEY BROOK BRIDGE**  
IN THE TOWN OF  
**FORT KENT**  
**AROOSTOOK COUNTY**  
SURVEY

SHEET 1 OF 3 AUGUSTA, MAINE JANUARY, 1965

M-2528







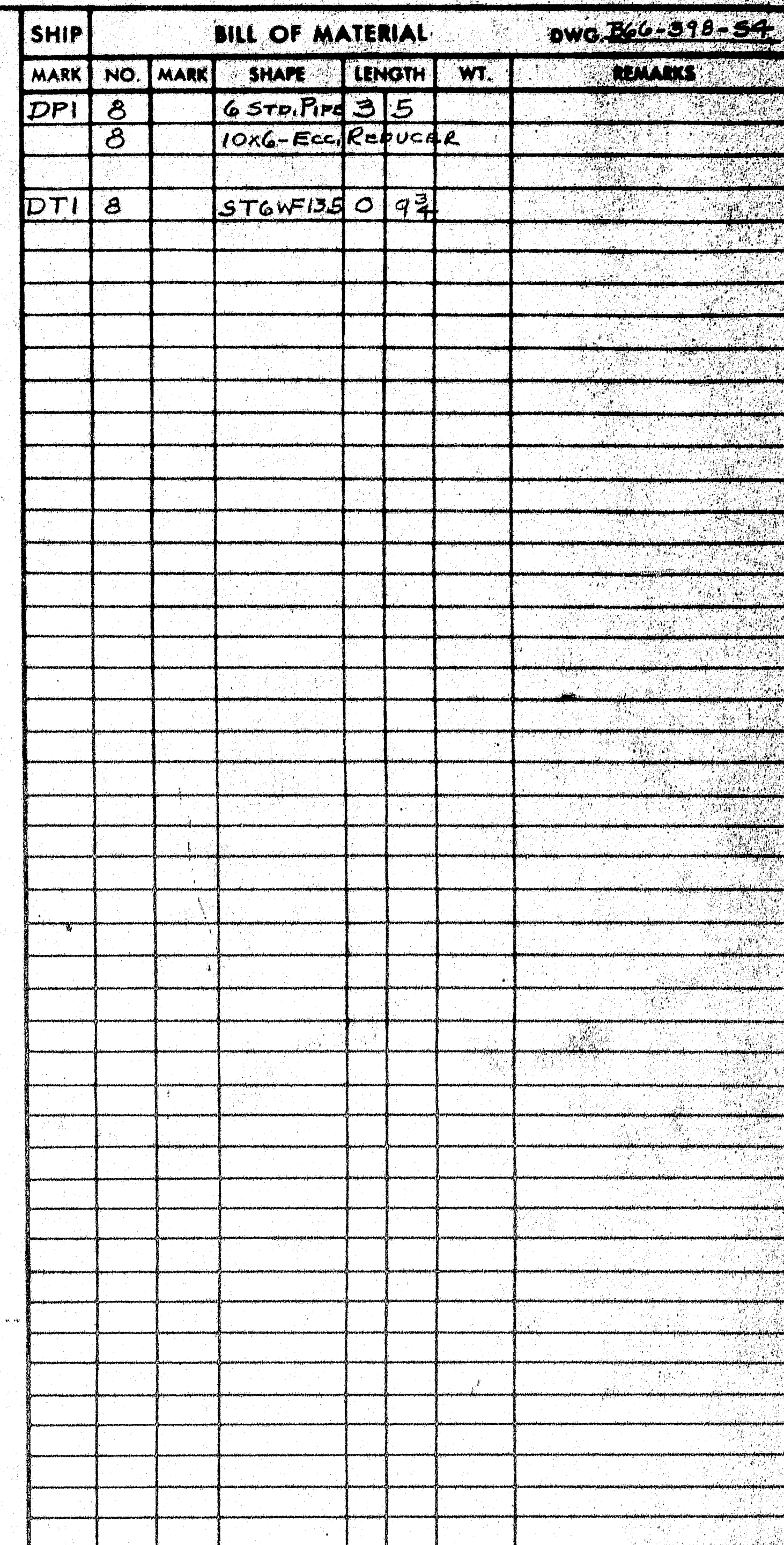


4-PORT. 1-25-67	I 75 OVER RTE. 158
5-DIST. 1-25-67	SHERMAN Ma
2-FAL. 1-12-67	CUSTOMER CALLAHAN BROS.
DRAGON 1-11-67	WHEELWRIGHT MS. MC BRIDE & CO.
REYNOLD	UNDER NO. VERBAL
REYNOLD	
REYNOLD	









SHOP CONNECTIONS: WELD  
FIELD CONNECTIONS: WELD  
HOLES: AS NOTED  
PAINT: STATE OF MAINE SPECS.

Project No I-95-9 (34)

## BRIDGE DRAINS

PRINT ISSUE

*Bancroft & Martin Inc.*  
*Brewer, Maine*

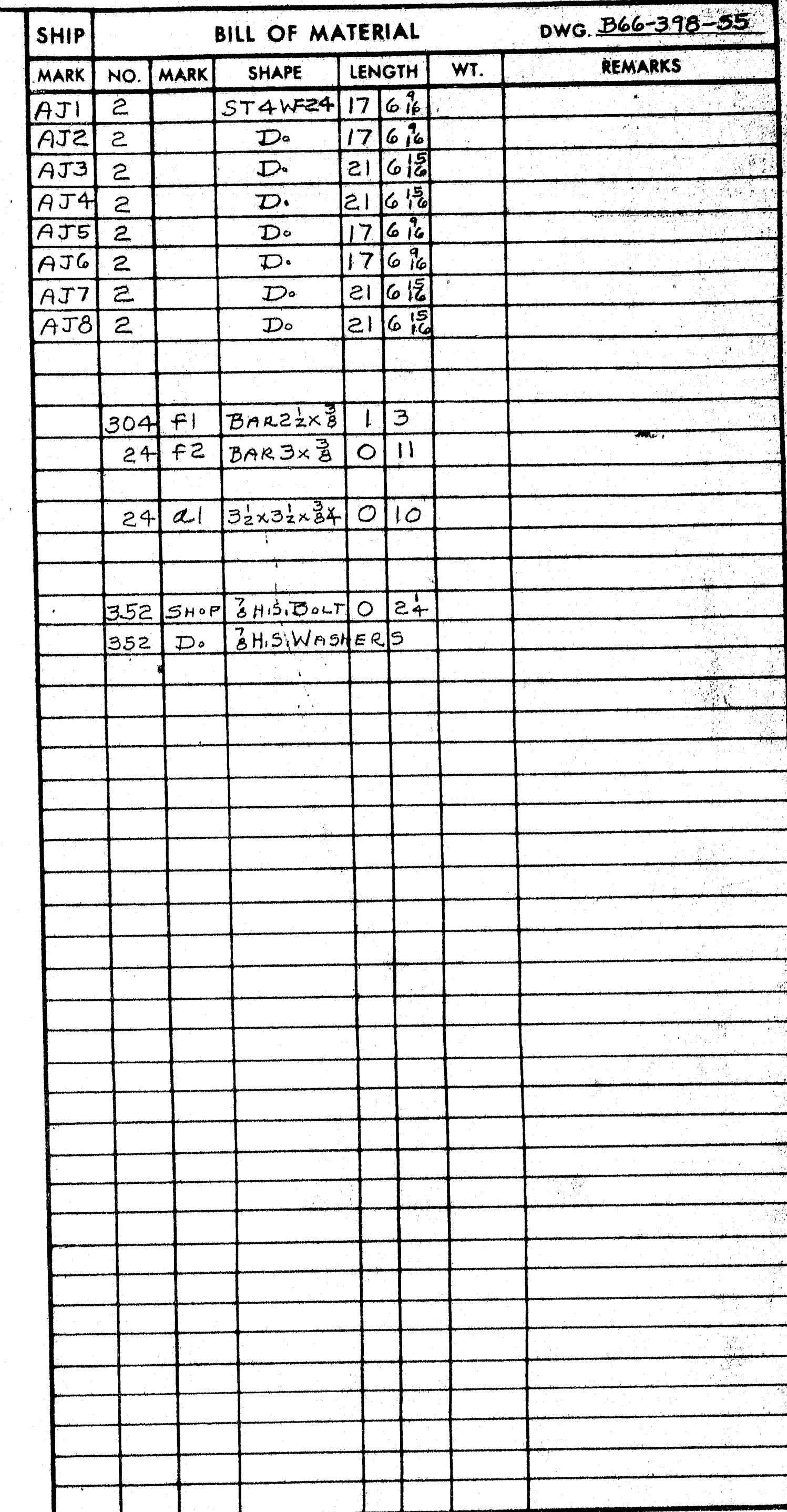
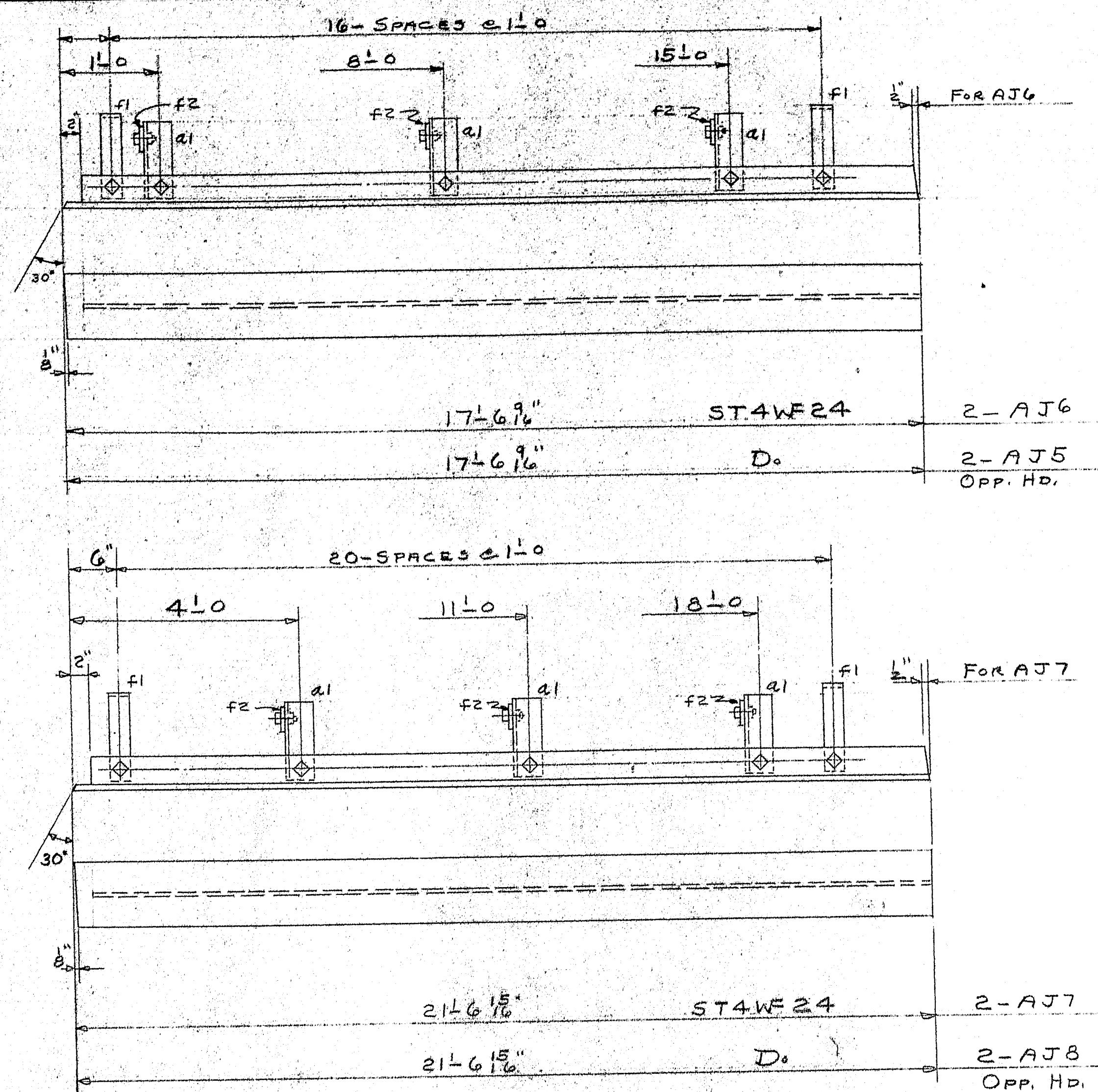
I95 OVER RTE. 158  
SHERMAN ME.

CUSTOMER CALLAHAN BROS.  
DESIGNER STATE HIGHWAY COMM.

ORDER <u>VERBAL</u>	DWG. <u>B66-398-S4</u>
---------------------	------------------------

6 SHOP	1-25-67
5 DIST	1-25-67
2 F.A.	1-12-67
DRAWN	1-11-67 D.C.
REVISION	-
REVISION	
REVISION	



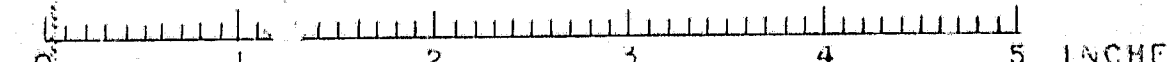


SHOP CONNECTIONS:  $\frac{7}{8}$  H.S. BOLTS  
FIELD CONNECTIONS: WELD  
HOLES:  $1\frac{5}{16}$ "  $\phi$  UNLESS NOTED  
PAINT: STATE OF ME. SPEC'S.

## ARMORED JOINTS

PRINT ISSUE			<i>Bancroft &amp; Martin Inc.</i> <i>Brewer, Maine</i>	
			I 95 OVER RTE. 158 SHERMAN ME.	
6 SHOP	1-25-67		CUSTOMER <u>CALLAHAN BROS.</u> DESIGNER <u>STATE HIGHWAY COMM.</u>	
5 DIST.	1-25-67		ORDER <u>VERBAL</u> DWG. <u>B66-398-55</u>	
2 F.A.	1-12-67			
DRAWN	1-12-67	D.C.		
REVISION				
REVISION				
REVISION				

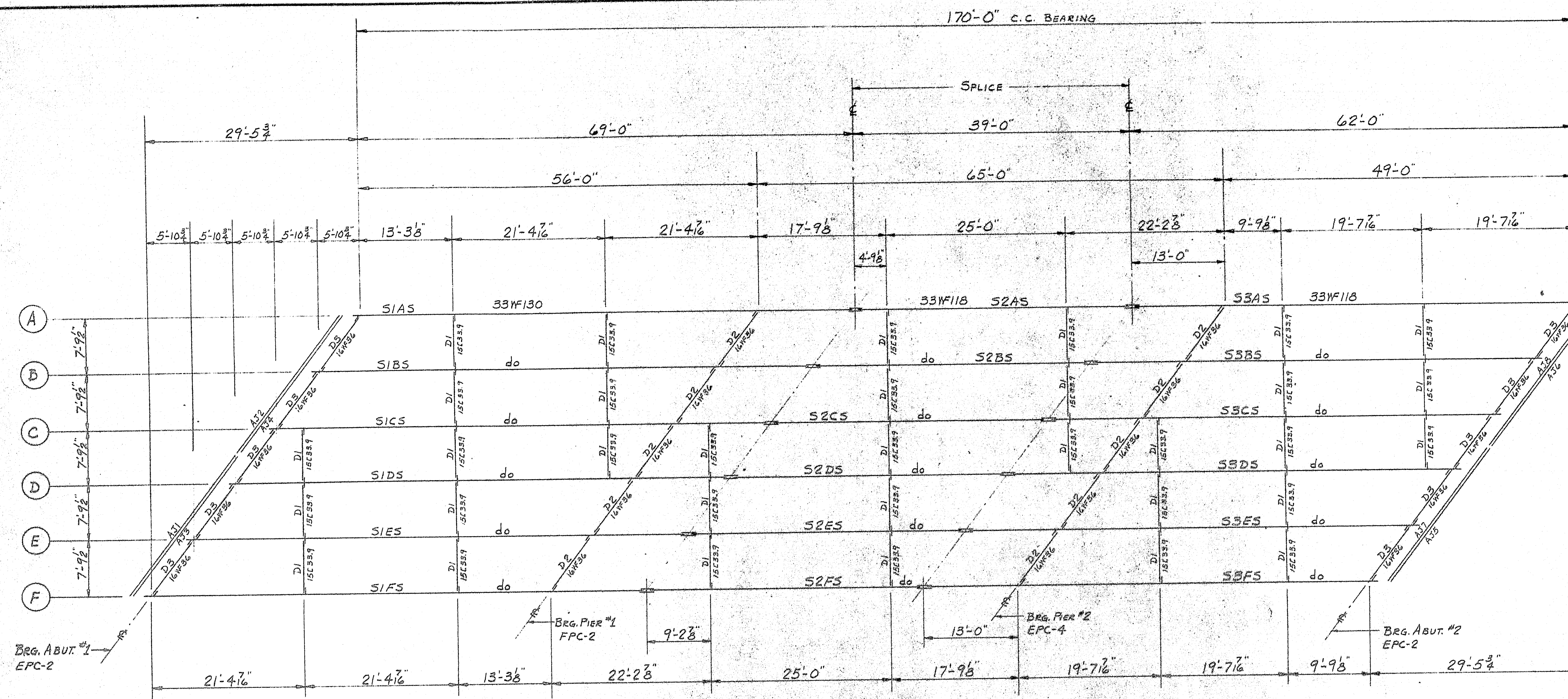
100-126



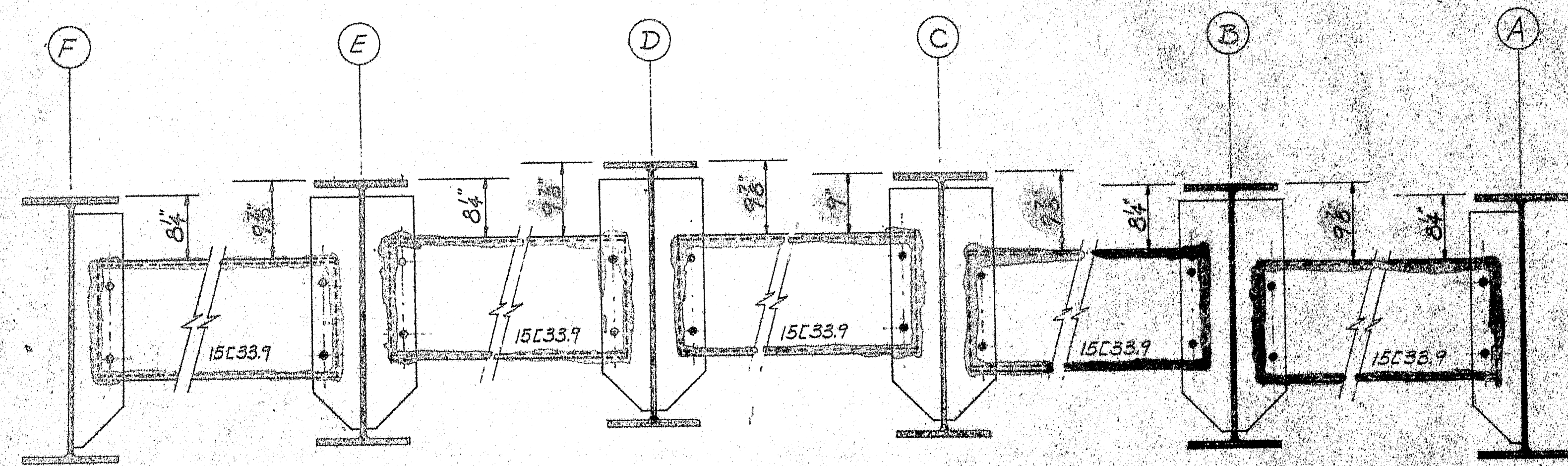




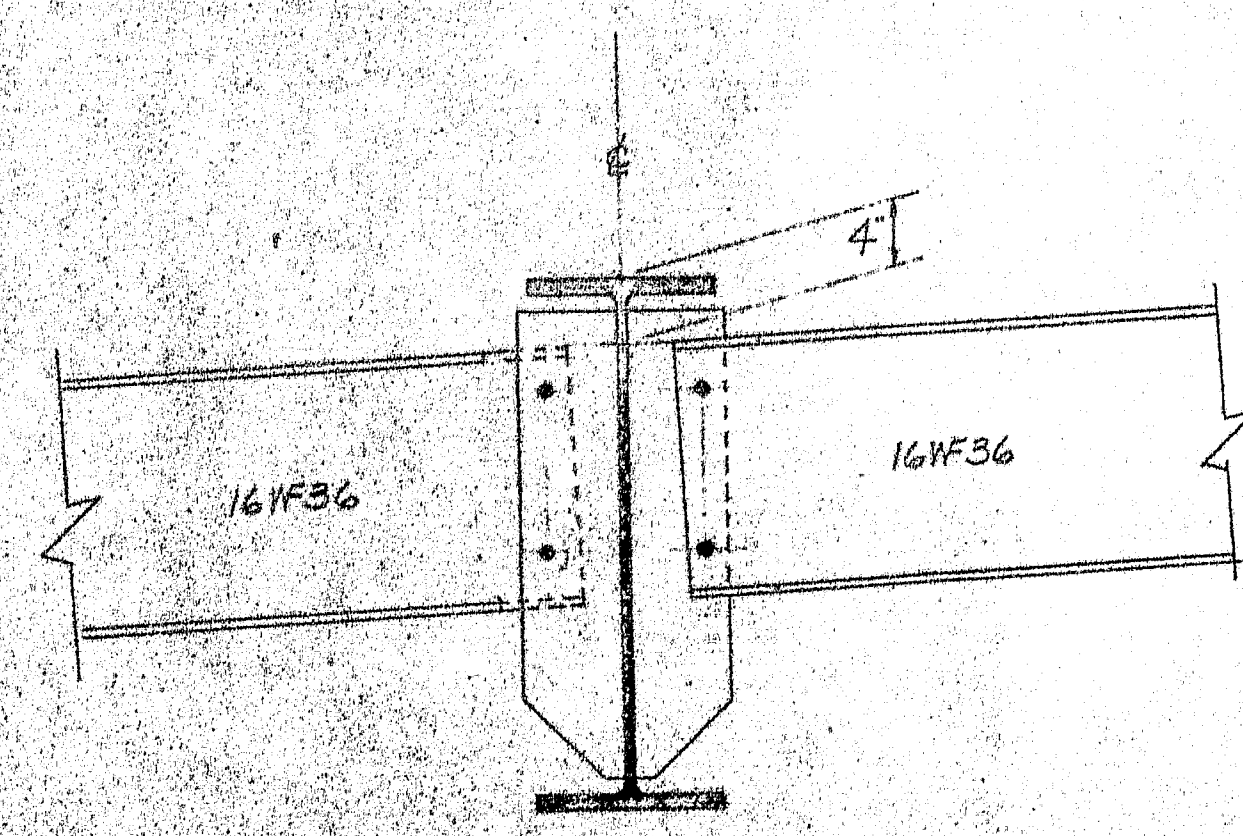




FRAMING PLAN  
SOUTHBOUND



DIAPHRAGM CONNECTION



DIAPHRAGM CONNECTION

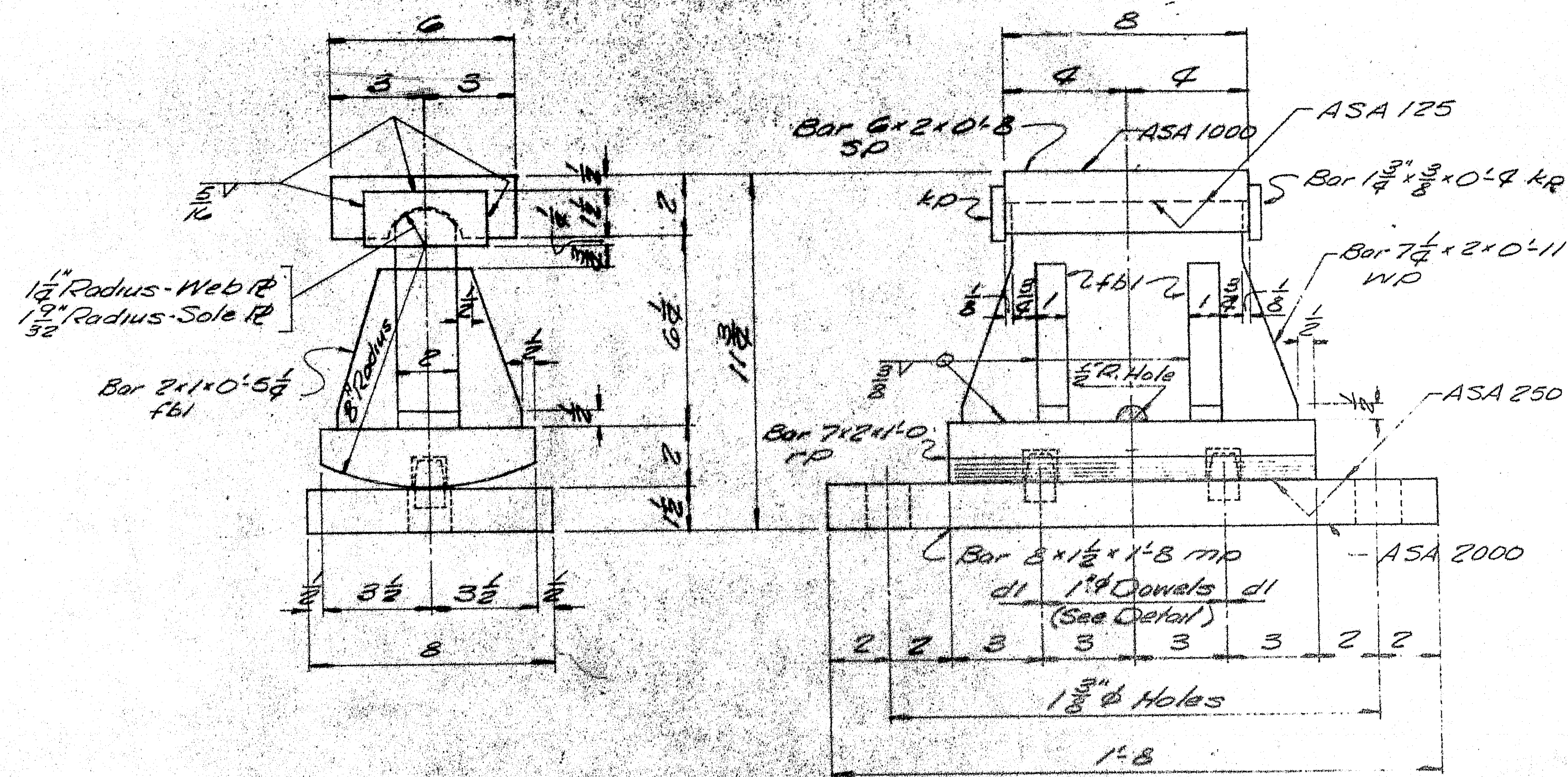
SHOP CONNECTIONS: WELD E70 LOW-HYDROGEN  
FIELD CONNECTIONS: 3/4 H.S. BOLTS  
HOLES: 1/8" UN.  
PAINT: STATE OF MAINE SPEC.

PROJ. NO. I-95-9(34)25B APP. AS NOTED 2-13-67

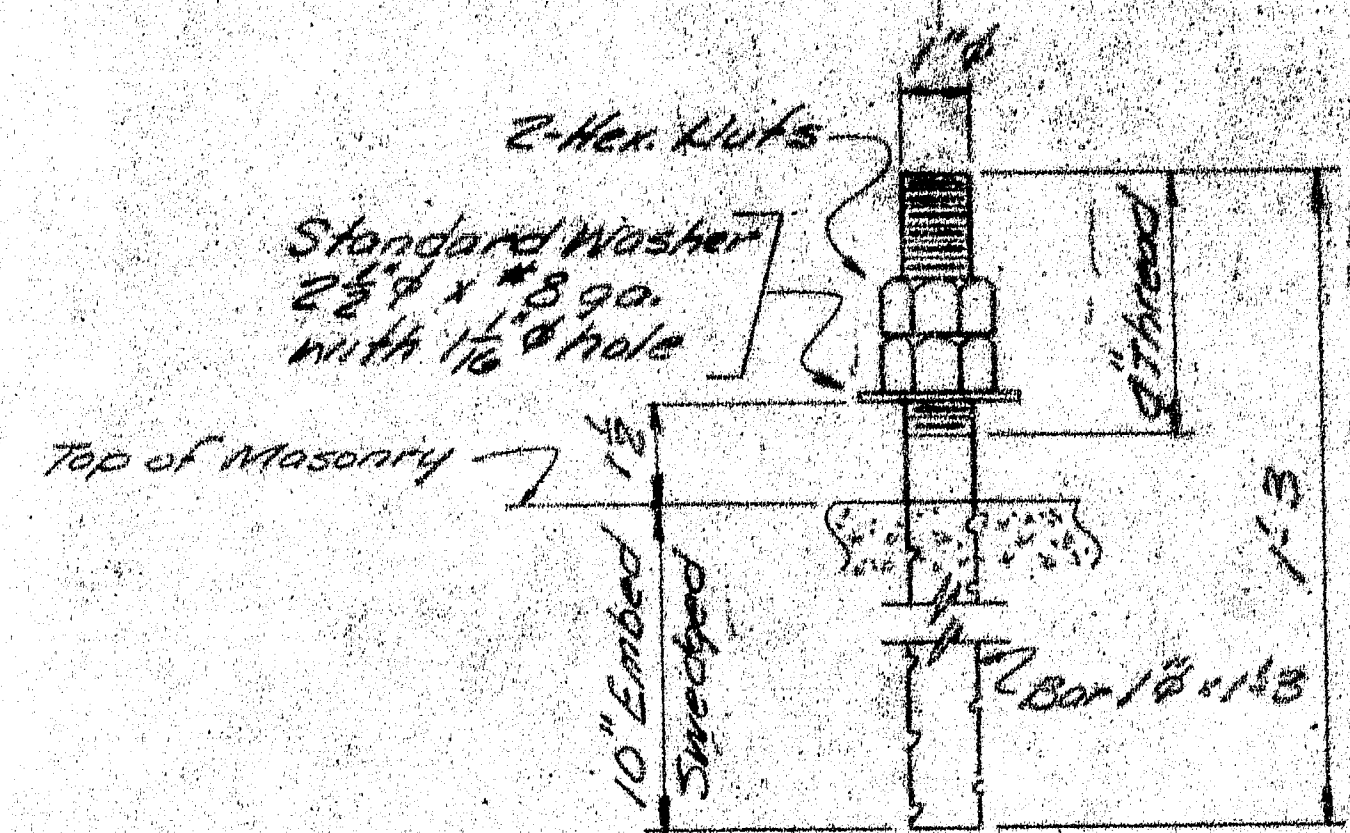
SOUTHBOUND FRAMING PLAN			Bancroft & Martin Inc. Brewer, Maine	
PRINT ISSUE			I-95 OVER BENEDICTA ROAD SHERMAN, MAINE	
2	S.H.C.	2-15-67	CUSTOMER: CALLAHAN BROS.	
3	CUST.	2-15-67	DESIGNER: M.S.H.C.	
2	SHOP	2-15-67	ORDER: VERBAL	
3	F.A.	1-30-67	DWG: B66-399-E2	
DRAWN	1-23-67 C.J.M.			
REVISION				
REVISION				
REVISION				

100-128

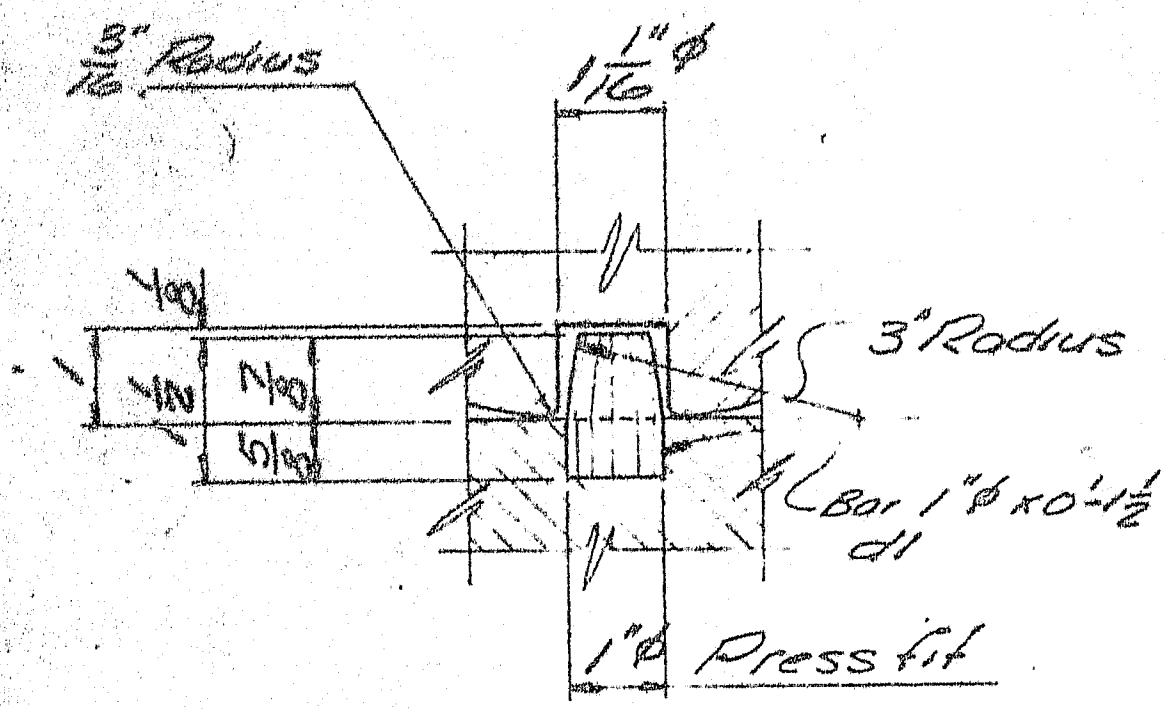




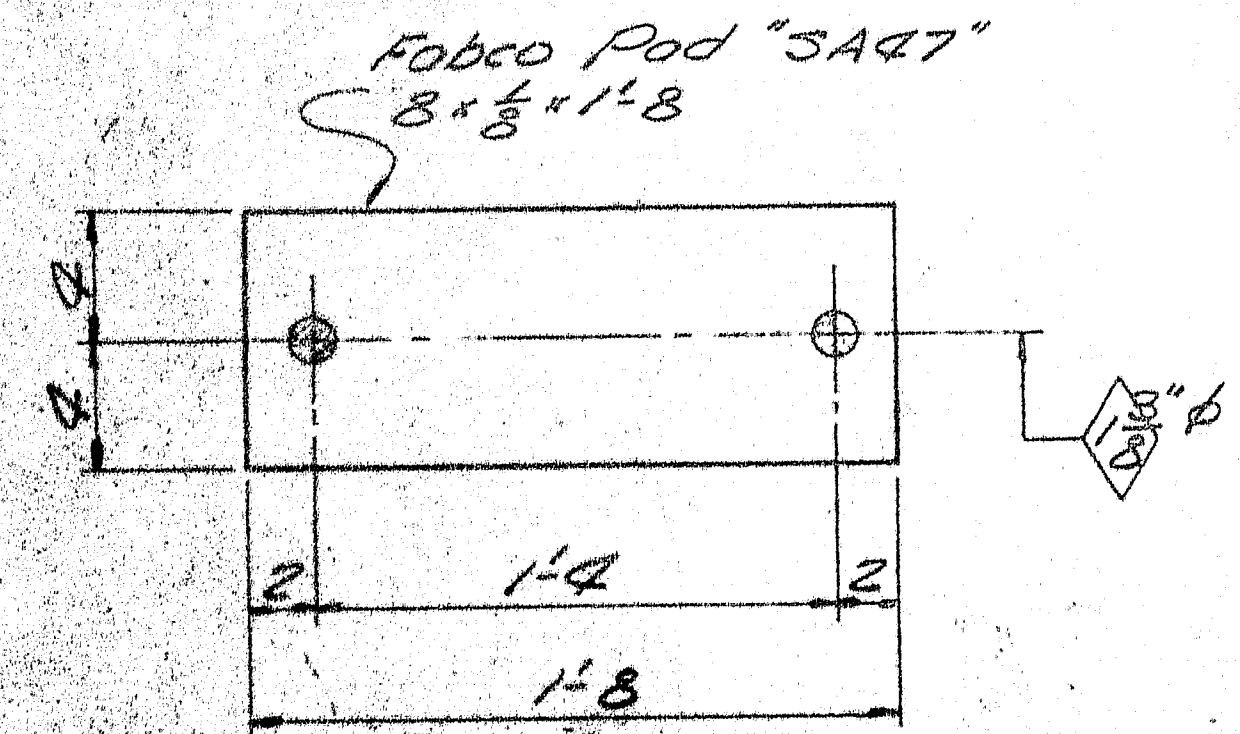
**EXPANSION PEDESTAL EPC-2**  
24 - REQ'D.



**ANCHOR BOLT-ABI**  
48 - REQ'D.



**DOWEL DETAIL**

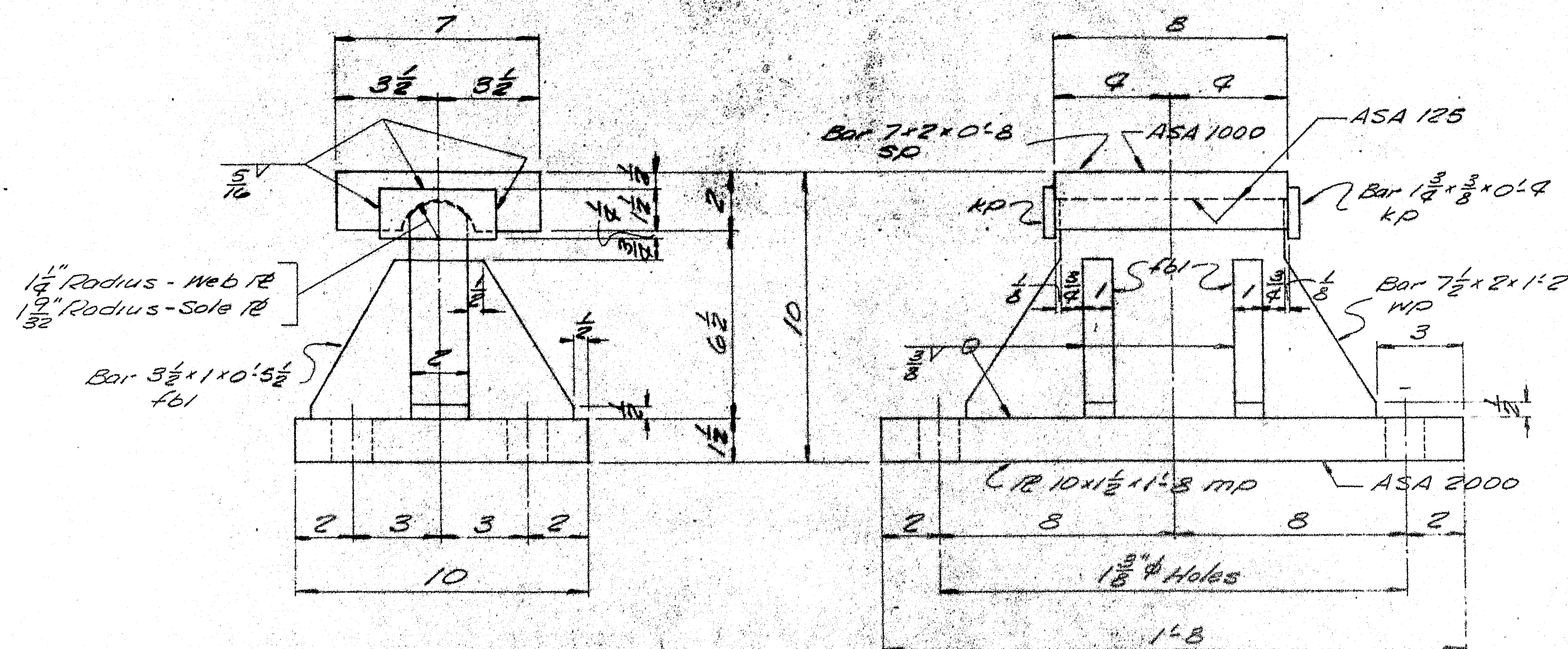


**FABCO "SA47" PAD**  
FPI 24 - REQ'D.

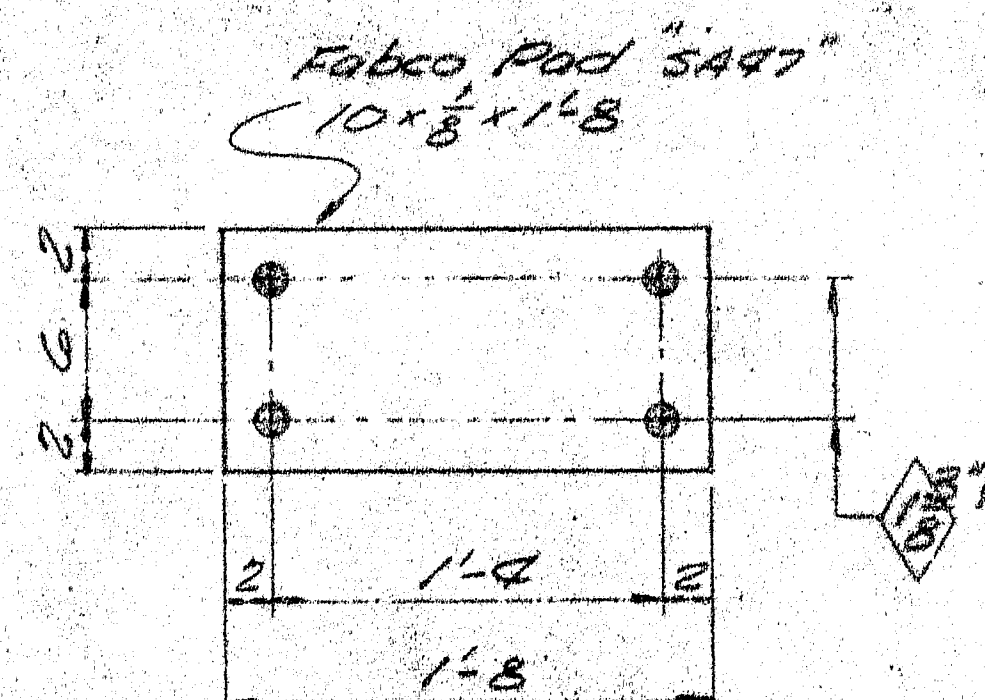
**PAINT NOTE:**  
No paint on top of sole plates "sp" and 1" down from top on sides, coat with boiled linseed oil.  
No paint on surface with ASA 125 and coat with mixture of white lead and tallow.  
No paint on Anchor bolts on top.

SHIP		BILL OF MATERIAL				DWG. NO. B66-397-51
MARK	NO.	MARK	SHAPE	LENGTH	WT.	REMARKS
EPC2	24		EXPANSION PEDESTAL ASSY.			
	24	mp	Bar 8x1 1/2	1 8		
	24	fp	Bar 7x12	1 0		
	24	wp	Bar 7 1/2 x 2	0 11		
	24	sp	Bar 6x2	0 8		
	96	fbi	Bar 2x1	0 54		
	48	di	Bar 1" phi	0 12		
	48	kp	Bar 1 1/2 x 1 1/8	0 4		
ABI	48		Bar 1" phi	1 3		Swaged
	96	shop	1" Hex. Nuts			
Field	48		1" Washer			Std. Washer 2 1/2" O.D. x 1 1/2" Bore with 1/8" hole
FPI	24		Pad 8x1 1/2	1 8		Fabco Pad "SA47" Req. No.
<p>Allowance to be made for Machining when cutting above plates.</p> <p>ITEM PROJECT NO. I-95-9(34) 258</p> <p>sole plates "sp" to be field welded to stringers.</p> <p>Bearing material to be ASTM-A36, Anchor Bolts to be A7, A36, or A307.</p> <p>All welds to be made with E70 Electrodes.</p> <p>SHOP CONNECTIONS: Welded</p> <p>FIELD CONNECTIONS: As Noted</p> <p>HOLES: As Noted</p> <p>PAINT: Red lead, per Maine S.H.C. Spec, and as noted.</p> <p>NORTHBOUND &amp; SOUTHBOUND</p> <p><b>BEARING PEDESTAL DETAIL</b></p> <p align="center">Banoroff &amp; Martin Inc. South Portland 7, Maine</p> <p align="center">I-95 OVER BENEDICTA ROAD SHERMAN, MAINE</p> <p align="center">CUSTOMER CALLAHAN BROS. DESIGNER M.S.H.C. BRIDGE DIV.</p> <p>2-S.H.C. 2-23-67 3-Cust. 11-18-66 4-F.B.T. 11-18-66 3-F.A. 11-16-66 DRAWN 11-16-66 C.J.M. REVISION REVISION</p> <p>ORDER NO. VERBAL DWG. NO. B66-397-51</p>						

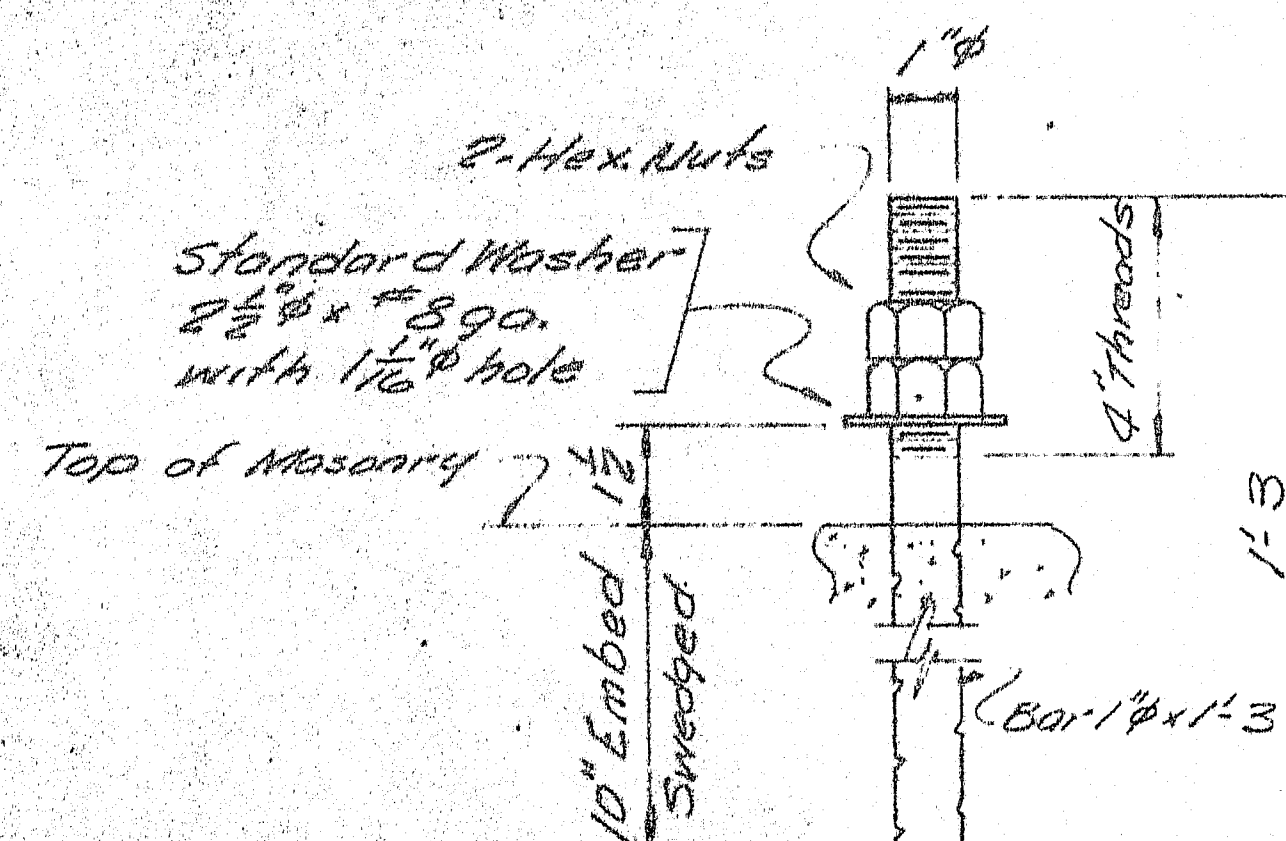




FIXED PEDESTAL FPC-2  
12 - REQ'D.



FABCO "SA47" PAD  
FP2 12 - REQ'D.



ANCHOR BOLT ABI  
48 - REQ'D.

PAIN'T NOTE:

No paint on top of sole plates "sp" and 1" down from top on sides, coat with boiled linseed oil.  
No paint on surface with ASA 125 finish, coat with mixture of white lead and tallow.  
No paint on Anchor bolts - Oil them.

SHIP		BILL OF MATERIAL				DWG. NO. B66-39952
MARK	NO.	MARK	SHAPE	LENGTH	WT.	REMARKS
FPC2	12		FIXED PEDESTAL ASSY.			
	12	MD	10 1/2 x 10 1/2	1	8	
	12	MD	Bar 7/8 x 2	1	2	
	12	SP	Bar 7/8 x 2	0	8	
	24	KP	Bar 1 1/8 x 3/8	0	2	
	48	FBI	Bar 3 1/2 x 1	0	5 1/2	
ABI	48		Bar 1" x 1"	1	3	Swaged
	96	shop	1" Hex. Nut			
Field	48		1" Washer			Std. washer 2 1/2" O.D. 1" Bore with 1/16" hole
FP2	12		Pad 10 x 8	1	8	Fabco Pad "SA47" Reg. #
Allowance to be made for machining when cutting above plates.						
ITEM PROJECT NO. I-95-9 (34) 258						
Sole plates "sp" to be field welded to stringers.						
Bearing material to be ASTM A36, Anchor bolts to be A7, A36, or A307.						
All welds to be made with E70 Electrodes.						
SHOP CONNECTIONS: Welded						
FIELD CONNECTIONS:						
HOLES: As noted						
PAINT: Red lead per Maine S.H.C. spec, and as noted.						
NORTHBOUND & SOUTHBOUND						
BEARING PEDESTAL DETAIL						
Bancroft & Martin Inc.						
South Portland 7, Maine						
I-95 OVER BENEDICTA ROAD						
SHERMAN, MAINE						
CUSTOMER CALLAHAN Bros.						
DESIGNER M.S.H.C. BRIDGE DIV.						
ORDER NO. VERBAL						
DWG. NO. B66-39952						

2-S.H.C. 2-23-67

3-CUST. 11-18-66

4-PORT. 11-18-66

3-FA. 11-16-66

DRAWN 11-16-66 C.J.M.

REVISION

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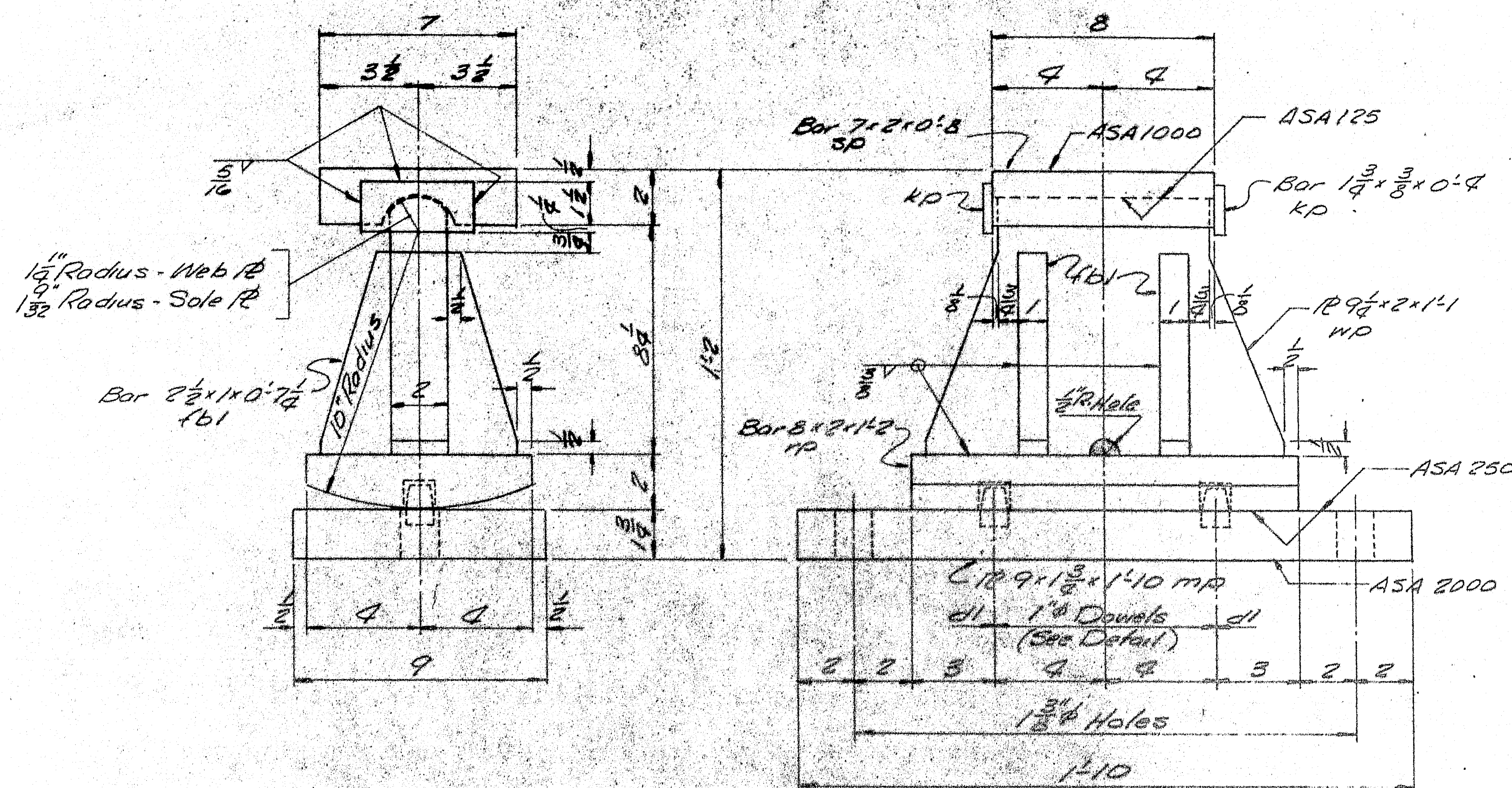
REVISION

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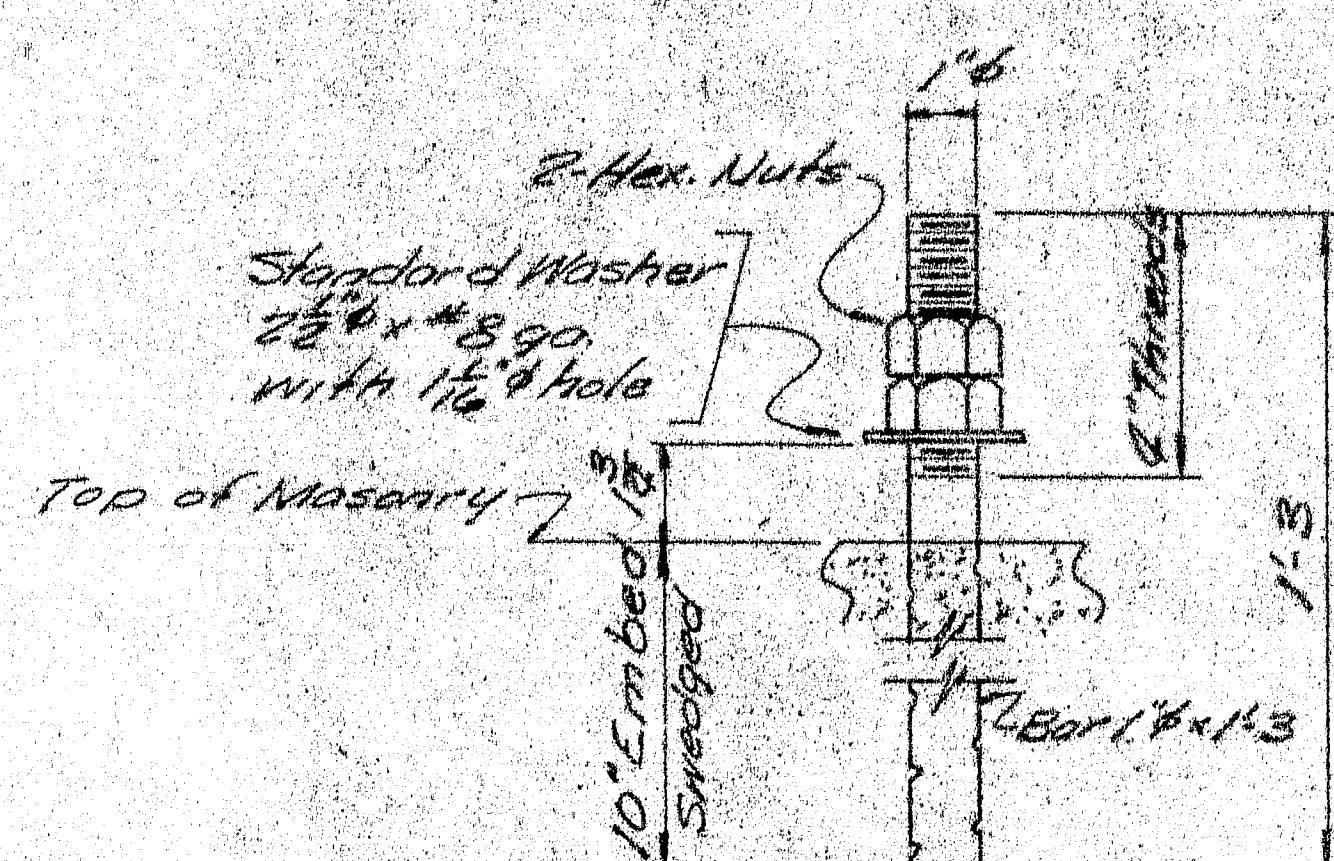
REVISION





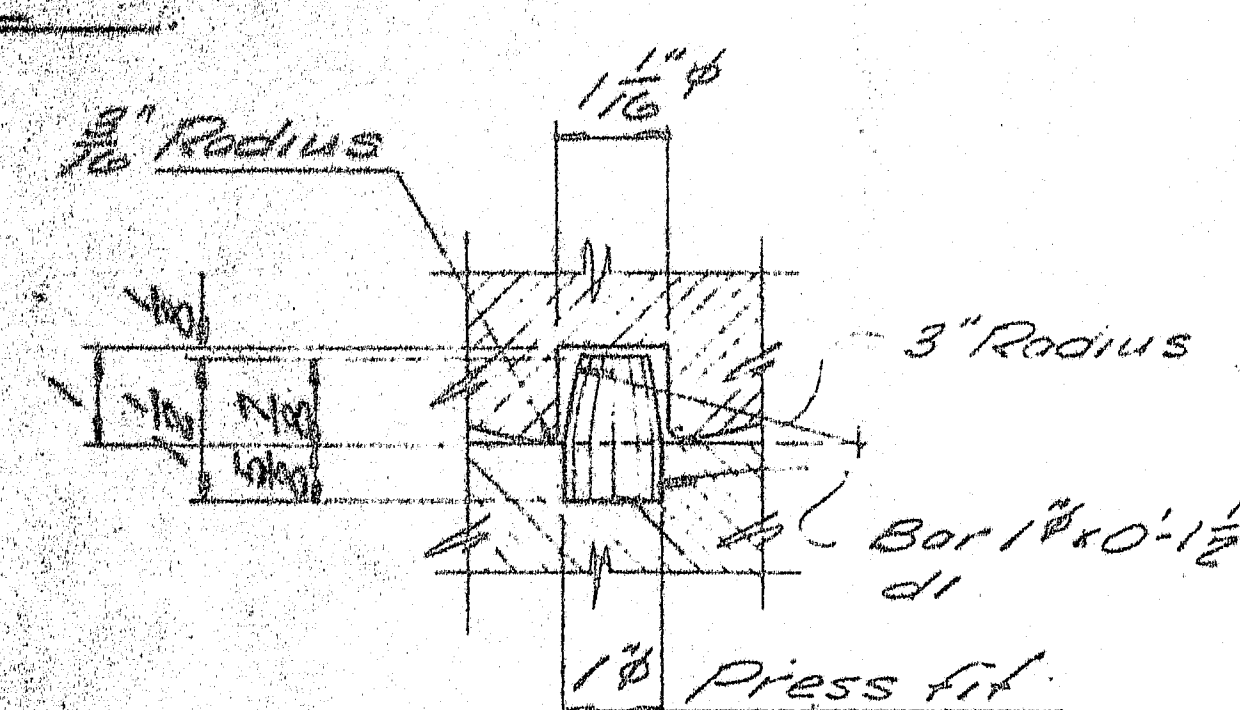
EXPANSION PEDESTAL EPC-4

12 - REQ'D.

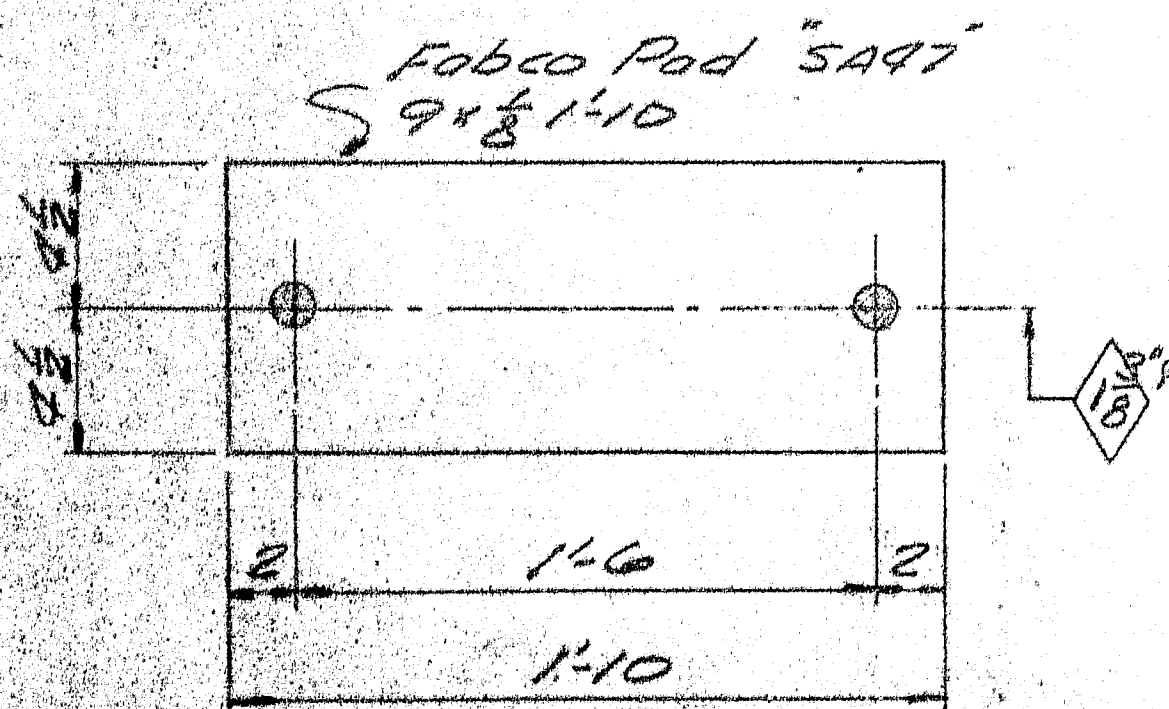


ANCHOR BOLT ABI

24 - REQ'D.



DOWEL DETAIL



FABCO PAD "SA47"

FP3 12-REQ'D.

PAINT NOTE:

No paint on top of sole plates "sp" and 1" down from top on sides, coat with boiled linseed oil.  
No paint on surface with ASA 126 finish, coat with mixture of white lead and tallow.  
No paint on Anchor Bolts - Oil them.

SHIP		BILL OF MATERIAL				DWG. NO. B66-399-53
MARK	NO.	MARK	SHAPE	LENGTH	WT.	REMARKS
ERC-4	12		EXPANSION PEDESTAL ASSY.			
	12	MD	R9x1.3	1	10	
	12	1P	Bar 8x2	1	2	
	12	WD	R9x2	1	1	
	12	SP	Bar 7x2	0	8	
	48	1b1	Bar 2x1	0	7.4	
	24	d1	Bar 1" d	0	1.2	
	24	KP	Bar 1.3x.8	0	4	
ABI	24		Bar 1" d	1	3	Swagged
	48	shop	1" Hex Nut			
Field	24		1" washer			Std. washer - 2 1/2" O.D. x 3/8" Bore with 1 1/2" hole
FP3	12		Red 9x.8	1	10	Fabco Pad "SA47" Req. 11b
<p>Allowances to be made for machining when cutting above plates.</p> <p>ITEM PROJECT NO. I-95-9(34) 258</p> <p>sole plates "sp" to be field welded to stringers.</p> <p>Bearing material to be ASTM-A36, Anchor bolts to be A7, A36, or A307. All welds to be made with E70 Electrodes.</p> <p>SHOP CONNECTIONS: welded</p> <p>FIELD CONNECTIONS:</p> <p>HOLES: As noted</p> <p>PAINT: Red lead per Maine S.H.C. Spec., and as noted.</p> <p>NORTHBOUND &amp; SOUTHBOUND</p> <p>BEARING PEDESTAL DETAIL</p> <p>Bancroft &amp; Martin Inc.</p> <p>South Portland 7, Maine</p> <p>I-95 OVER BENEDICTA ROAD SHERMAN, MAINE</p> <p>CUSTOMER: CALLAHAN BEGS</p> <p>DESIGNER: M.S.H.C. BRIDGE DIV.</p> <p>ORDER NO. VERBAL DWG. NO. B66-399-53</p>						
2-S.H.C. 2-23-67	3-Cust. 11-13-66	4-Rev. 11-13-66	3-FA. 11-16-66	DRAWN 11-16-66 C.J.M.	REVISION	REVISION

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